

NOTES :

1. Designations provided in parenthesis reference standard elements detailed in "A Guide to Standardized Highway Barrier Rail Hardware, 1979, AASHTO-MCC-ARTBA Joint Cooperative Committee.
2. No washers are used on the 3/4" diameter head bolts (F-3) (as required J-76) connecting the rail to the Centralized Releasing Terminal (CRT) posts.
3. The rail is bolted to the CRT post at the center of the nose as shown.
4. The curved guardrail section shall be shop bent.
5. The Special Anchor has not been tested as a crashworthy end treatment for approaching traffic on the inner section roadway. Therefore, its use shall be limited to driveways or service roadways paralleling drainage facilities.

Radius	No. of CRT Posts	Required Area Free of Fixed Objects
8'-6"	5	L W 25' x 15'
17'-0"	6	30' x 15'
25'-6"	8	40' x 20'
35'-0"	11	50' x 20'

Figure 1
 CURVED GUARDRAIL
 DETAIL
 (8'-6" radius)

SECTION B-B

SECTION A-A

NO.	DATE	PROJECT	BY	CHECKED

NOTES:

1. Designations provided in parenthesis reference standard elements detailed in "A Guide to Standardized Highway Barrier Roll Hardware," 1979, AASHTO-ACC-ART-BA Joint Cooperative Committee.
2. No washers are used on the $\frac{3}{4}$ " button head bolts (F-3103) required (J-16) connecting the roll to the Controlled Releasing Terminal (CRT) posts.
3. The curved guardrail section should be strap bent.
4. The Special Anchor has not been tested as a crashworthy and treatment for approaching traffic on the intersecting roadway. Therefore, its use shall be limited to driveways or service roadways paralleling drainage facilities.
5. Details for curved guardrail radii of 8'-6", 17', and 25'-6" are included with Figure 1.

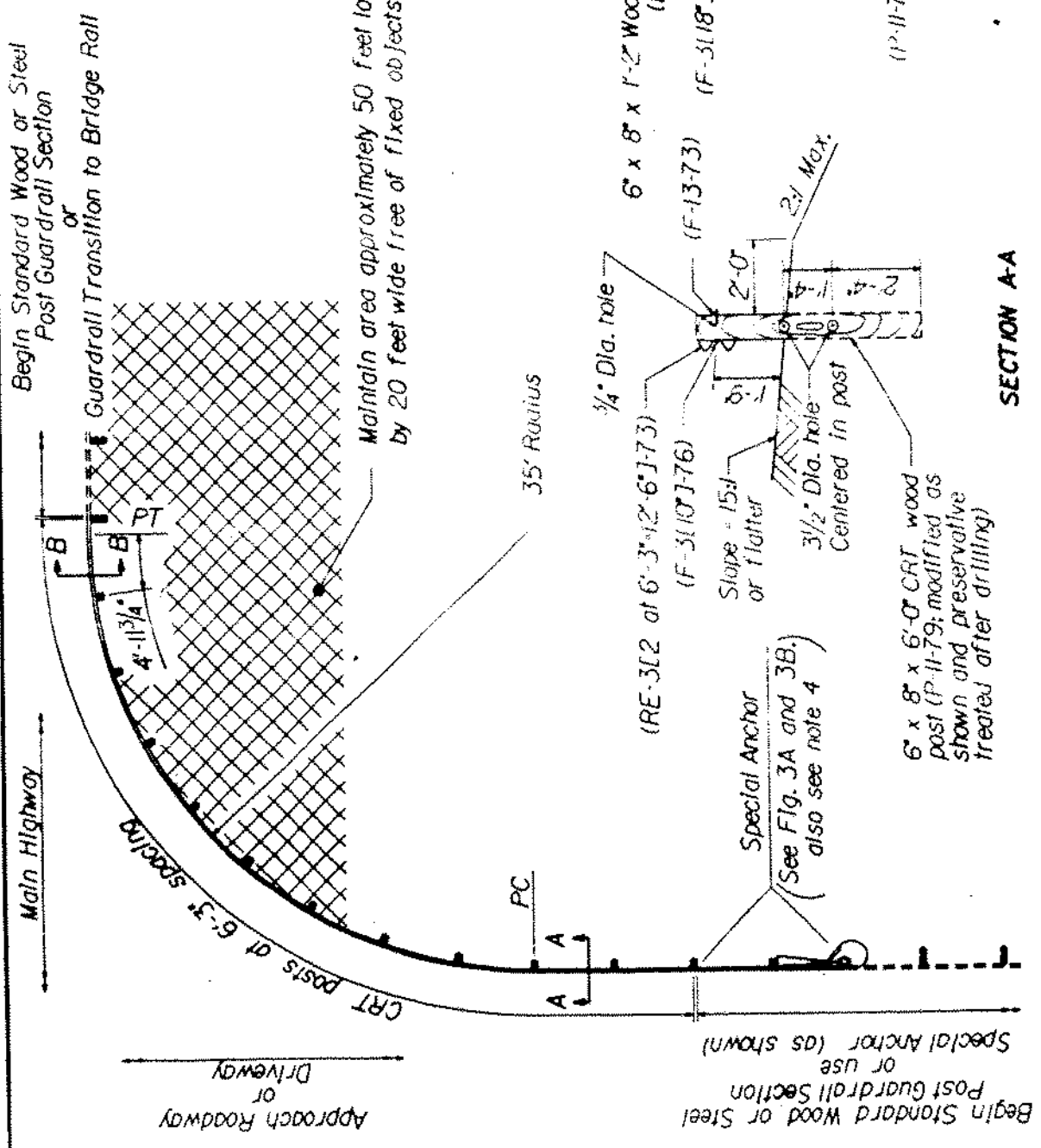


Figure 2
 CURVED GUARDRAIL
 DETAIL
 (35' radius)

NO.	DATE	PROJECT	BY

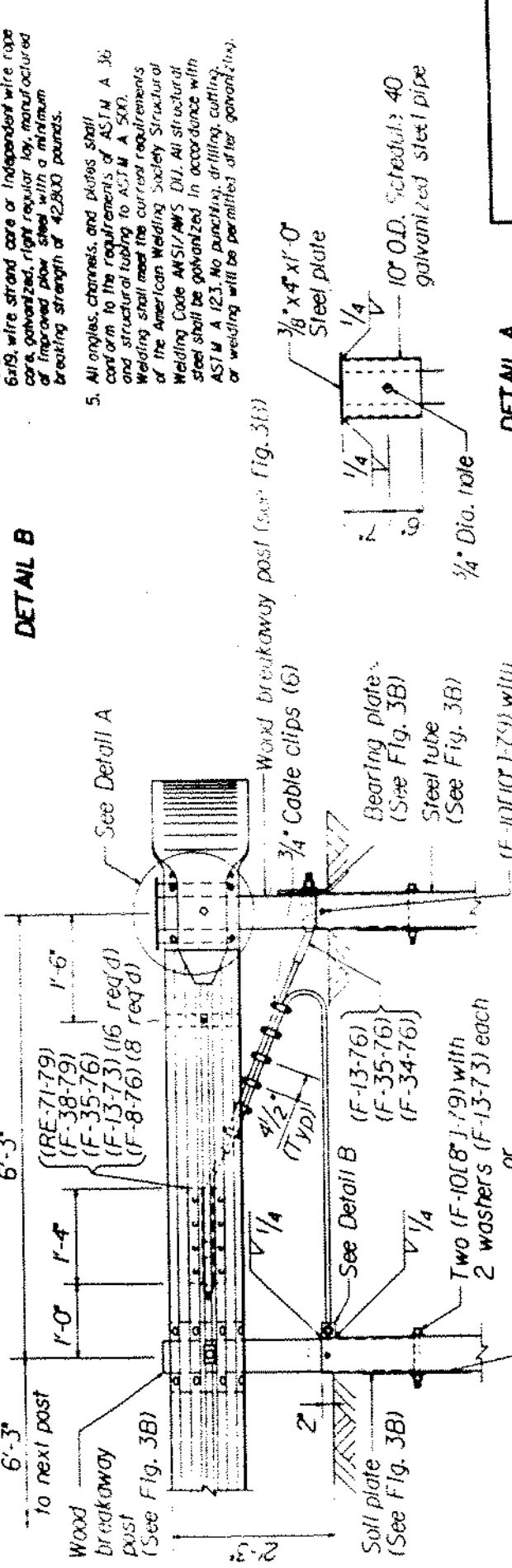
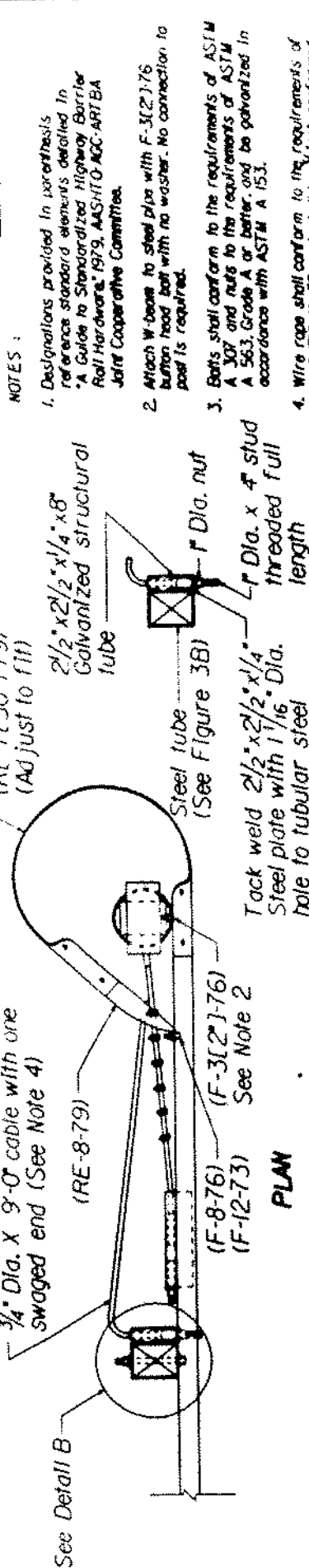


Figure 3A
CURVED GUARDRAIL
SPECIAL ANCHOR
DETAILS

DETAIL A

POST A

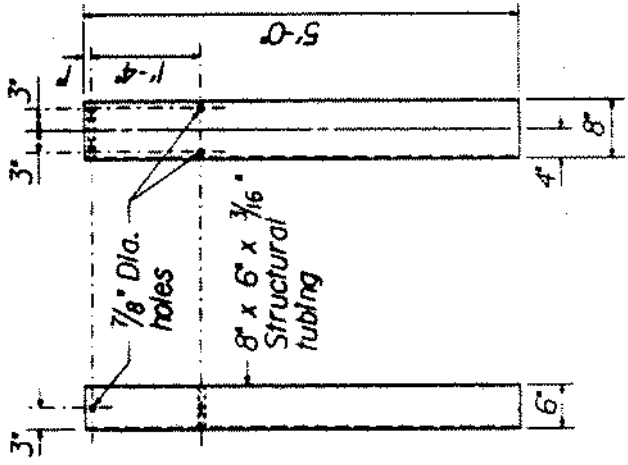
POST B

ELEVATION

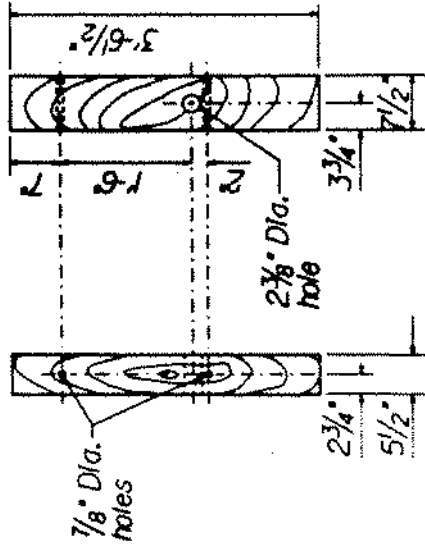
NO.	DATE	PROJECT	TOTAL SHEETS

NOTES :

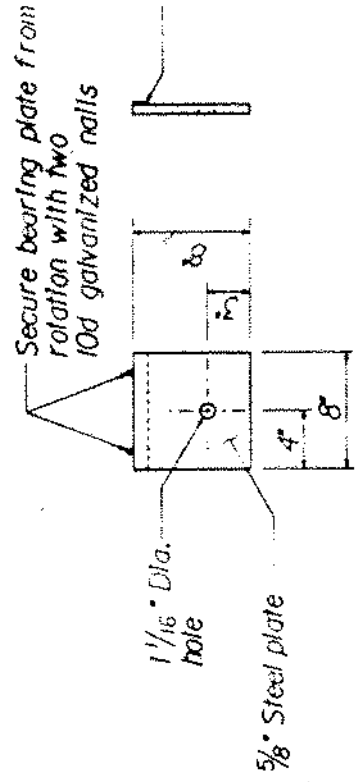
1. The wood breakaway post shall be S4S timber with a stress grade of 1200 psi and shall be grade marked or certified by a recognized association or agency which is certified by the Board of Review, American Lumber Standards Committee, to grade the species. It shall receive a preservative treatment in accordance with AASHTO designation M 133.
2. All angles, channels, and plates shall conform to the requirements of ASTM A 36 and structural tubing to ASTM A 500. Welding shall meet the current requirements of the American Welding Society Structural Welding Code AWS/AWS D11. All structural steel shall be galvanized in accordance with ASTM A 123. No punching, drilling, cutting, or welding will be permitted after galvanizing.
3. Post B is fabricated from Post A by adding the galvanized structural tube (See Figure 3A, Detail A) before galvanizing.



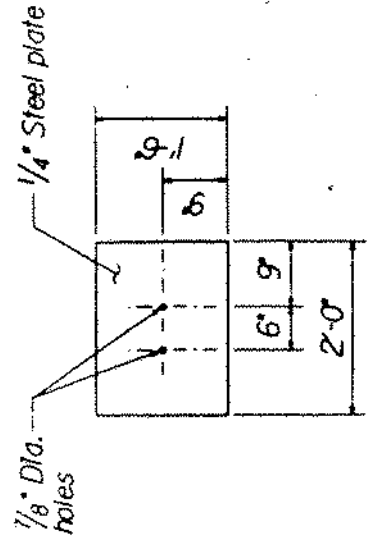
STEEL TUBE (POST A)



WOOD BREAKAWAY POST



BEARING PLATE



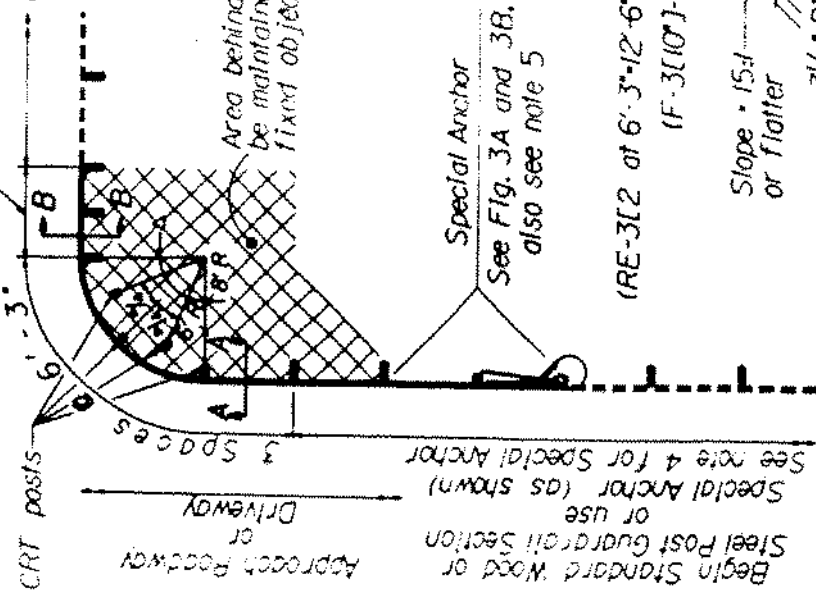
SOIL PLATE

**Figure 3B
CURVED GUARDRAIL
SPECIAL ANCHOR
DETAILS**

Main Highway - Maximum 50 MPH

2 spaces @ 3'-1/2" x 6'-3"

Begin Standard
Guardrail Transition to Bridge Roll



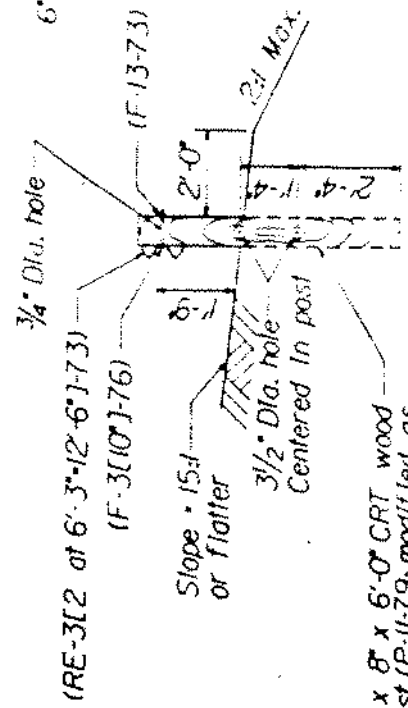
NOTES:

1. Designations provided in parenthesis reference standard elements detailed in "A Guide to Standardized Highway Barrier Rail Hardware," 1979, AASHTO-AEC ART BA Joint Cooperative Committee.
2. No washers are used on the 3/4" diameter bolts (F-3[107]-76) connecting the rail to the Centralized Releasing Terminal (CRT) posts.
3. The curved guardrail section shall be shop bent.
4. The Special Anchor has not been tested as a crashworthy end treatment for approaching traffic on the lane sealing roadway. Therefore, its use shall be limited to driveways or service roadways paralleling drainage facilities, with low speeds and very low traffic volumes.

Special Note:
This system is intended for use in the very specific situation of intersecting roadways directly adjacent to a canal or other feature running parallel to the main approach which requires a bridge on the main roadway.
This system should be used when low deflection rates are required, and on low speed (50 MPH maximum) main roadways.

Special Anchor
See Fig. 3A and 3B, also see note 5

Notes: If no special anchor is used, use two (P-11-79) blocked out wood posts spaced at the standard 6'-3" spacing.



6' x 8' x 6'-0" CRT wood post (P-11-79; modified as shown and preservative treated after drilling)

SECTION B-B

SECTION A-A

Figure 4
YUMA COUNTY, AZ
CURVED GUARDRAIL
DETAIL
(8'-6" radius)