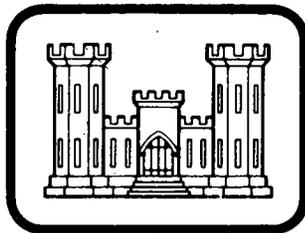


FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT

ORLEANS PARISH, LOUISIANA
RELOCATION OF FACILITIES
DESIGN MEMORANDUM NO. 73



**United States Army
Corps of Engineers**

*... Serving the Army
... Serving the Nation*

DEPARTMENT OF THE ARMY
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS
NEW ORLEANS, LOUISIANA

JUNE 1983

Serial No. 27

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DEPARTMENT OF THE ARMY
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS
P. O. BOX 60267
NEW ORLEANS, LOUISIANA 70160



REPLY TO
ATTENTION OF:

LMNED-SR

17 JUN 83

SUBJECT: Flood Control, Mississippi River and Tributaries,
Mississippi River Levees, Item M-94.3-R, Algiers Point
Setback and Levee Enlargement, Louisiana, Relocation of
Facilities, Design Memorandum No. 73

President
Mississippi River Commission
ATTN: MRCE-D

1. The subject design memorandum is submitted herewith for review and approval in accordance with the provisions of ER 1110-2-1150 dated 2 Aug 76, and LMVD Supplement 1, dated 1 October 1981.
2. The final Environmental Impact Statement (EIS) for the Mississippi River Levees and Channel Improvement projects, which includes the Algiers Point Levee Setback, was filed with the Council on Environmental Quality on 8 April 1976 and the notice of its availability was published in the Federal Register on 16 April 1976.
3. Based on studies and investigations at this stage of design, the proposed action is not likely to jeopardize the continued existence of any endangered species or threatened species or result in the destruction or adverse modification of critical habitat of such species.
4. The project is located within the Algiers Point Historic District. A historic overview conducted under contract to the New Orleans District indicated the presence of 19th century historic remains within the project area. We are in the process of coordinating a Memorandum of Agreement with the State Historic Preservation Officer and the Advisory Council on Historic Preservation. Archeological excavations, designed to mitigate project impacts are scheduled to be completed by 20 December 1983. Upon completion of the field investigation we will prepare

LMNED-SR

17 JUN 80
SUBJECT: Flood Control, Mississippi River and Tributaries,
Mississippi River Levees, Item M-94.3-R, Algiers Point
Setback and Levee Enlargement, Louisiana, Relocation of
Facilities, Design Memorandum No. 73

and coordinate a written report on our findings. Additionally, specifications covering the work to be performed in constructing and maintaining the levees provide for preservation of any items of apparent historical or archeological interest which may be discovered in the course of construction activities. The specifications will require immediate notification of proper authorities if any items of this nature are encountered, and will further require that the work cease until the items can be classified and salvaged, if salvage is considered appropriate.

5. The Section 404 public notice for the levee district containing this item of work was issued on 20 October 1976. No adverse comments were received. In compliance with applicable regulations pursuant to Section 404(b) of Federal Water Pollution Control Act, a statement of findings covering proposed future construction of Mississippi River levees within the New Orleans District was submitted to the Environmental Protection Agency (EPA) on 14 July 1978. The statement of findings was approved on 6 November 1978. The subject work item has been reviewed with respect to useable data, and the 24 Dec 1980 EPA Guidelines. Since no significant impacts are expected to occur the work is considered to be in compliance with Section 404 of the Clean Water Act.

6. Section 111 of Public Law 85-500, as amended by Section 309 of Public Law 89-298 is considered as authority for relocation of the Algiers Point Brake Tag Station owned by the City of New Orleans. Therefore, subject Design Memorandum is submitted pursuant to paragraph 73-703 of ER 1180-1-1, and paragraph 21. b(2)(e), of ER 1110-2-1150.

7. The submittal of this design memorandum is in accordance with the existing project schedule which provides for relocation expenditures to begin in the second quarter of fiscal year 1984.

8. Approval of the design memorandum is recommended.

1 Incl (12 sets)
Design Memorandum No. 73

FDR:

Charles E. Deese
ROBERT C. LEE
Colonel, CE
Commanding

COL, CE
DPT

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES
WESTBANK ORLEANS PARISH
LOUISIANA

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT

RELOCATION DESIGN MEMORANDUM NO. 73

FOREWORD

During 1980, hydrographic surveys indicated a deepening of the river adjacent to Algiers Point. The levee alinement in the plans and specifications approved 27 May 1976 was not safe with the changed river conditions and studies were initiated to provide a new levee alinement. Design Memorandum No. 73 presents the results of this restudy as it requires the relocation, abandonment, alteration, or substitution of facilities impacted by the proposed project.

Department of the Army
New Orleans District, Corps of Engineers
New Orleans, Louisiana

June 1983

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2	TYPICAL SECTIONS
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8	MISC. DETAILS
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APPENDIXES

I	ATTORNEY'S REPORT
II	DETAILED COST ESTIMATES
III	SPECIAL STATUTORY AUTHORITY DATA

STATUS OF DESIGN MEMORANDUMS

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
1	Flood Control, Mississippi River and Tributaries, Item M-25.0-R, Buras Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	23 Nov 70
2	Flood Control, Mississippi River and Tributaries, Item M-26.0-R, Upper Buras Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	21 Jan 71
3	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-14.9-R, Commander Levee Enlargement, Plaquemines Parish, Louisiana Relocation of Facilities	11 Jun 71
4	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-10.7-R, Venice Levee Enlargement and Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	6 Apr 71
5	Lower Red River - South Bank Red River Levees, 22 Jan 71 (A) Item R-117.0-R (1957 Mileage), Levee Enlargement, Rapides-Cotton Bayou Levee, Rapides Parish, Louisiana, Relocation of Facilities	22 Jan 71
6	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-31.3-R, Tropical Bend Levee Setback, Plaquemines Parish, Louisiana Relocation of Facilities	30 Mar 71
7	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-10.4-R, Lower Venice Levee Enlargement and Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	27 Aug 71

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
8	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-18.9-R, Fort Jackson-Boothville Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	30 Nov 72
9	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-21.5-R, Childress Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	23 Sep 71
10	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-23.2-R, Buras-Triumph Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	31 May 73
11	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-55.3-R, Upper Junior Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	21 Jun 71
12	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-51.0-L, Gravolet Levee Enlargement and Setback, Plaquemines Parish, Louisiana Relocation of Facilities	14 Sep 72
13	Flood Control, Mississippi River and Tributaries, Lower Red River - South Bank Red River Levees, Item R-123.5-R (1957 Mileage), Scott Home-Bertrand Levee Enlargement, Rapides Parish, Louisiana, Relocation of Facilities	15 Sep 72
14	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-96.9-R, Amelia Street Levee, Jefferson Parish, Louisiana, Relocation of Facilities	31 Aug 71

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
15	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-41.7-R, Port Sulphur Levee Enlargement and Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	28 Jan 74
16	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-33.4-R, Nairn Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	22 Dec 75
17	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-37.7-R, Homeplace Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	5 Jan 73
18	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-57.7-R, Myrtle Grove Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	31 Jan 73
19	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-28.0-R, Empire Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	30 Aug 74
20	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-49.5-R, Woodland Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	24 Aug 73
21	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-47.2-R, Nolan Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	29 Jun 73

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
22	Flood Control, Mississippi River and Tributaries, Mississippi River Levee, Item M-45.0-R, Socola Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	28 Nov 72
23	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-75.0-L, Scarsdale Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	7 Dec 73
24	Flood Control, Mississippi River and Tributaries, Mississippi River levee, Item M-89.5-R, Cutoff Levee Setback, Orleans Parish, Louisiana, Relocation of Facilities	20 Dec 74
25	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-130.0-L, Gypsy Levee Setback, St. Charles Parish, Louisiana, Relocation of Facilities	30 Dec 74
26	Flood Control, Mississippi River and Tributaries, Atchafalaya River Levees, Item A-31.3-L, Cross Bayou Levee Setback, Pointe Coupee Parish, Louisiana, Relocation of Facilities	8 Nov 74
27	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M- 39.0-R, Freeport Levee Enlargement, Plaquemines Parish, Louisiana	27 Dec 74
28	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-30.2-R, Upper Empire Levee Enlargement and Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	30 Apr 75

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
29	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-63.0-L, Monsecour Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	15 Oct 75
30	Flood Control, Mississippi River and Tributaries, Atchafalaya River Levees, Item A-25.5-R, Goudeau Levee Setback, St. Landry Parish, Louisiana, Relocation of Facilities	30 Jun 76
31	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-320 to 303-R, Fifth Louisiana Levee District Levee Enlargement, Concordia, West Feliciana, and Pointe Coupee Parishes, Louisiana, Relocation of Facilities	30 Jun 75
32	Flood Control, Mississippi River and Tributaries, Atchafalaya River Levees, Item A-4.6-L, Legonier Levee Setback, Pointe Coupee Parish, Louisiana, Relocation of Facilities	30 Sep 75
33	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-35.0-R, Nairn Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	22 Dec 75
34	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-99.0-L, Louisiana Avenue Floodwall, Orleans Parish, Louisiana, Relocation of Facilities	21 Oct 75
35	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-144.5-L, Angelina Levee Setback, St. John the Baptist Parish, Louisiana, Relocation of Facilities	12 Dec 75

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
36	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-205.0-R, Point Pleasant Levee Setback, Iberville Parish, Louisiana, Relocation of Facilities	29 Nov 76
37	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-58.0-L, St. Sophie Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	15 Sep 75
38	Flood Control, Mississippi River and Tributaries, Atchafalaya River Levees, Item A-38.0-L, Holloway Lake Levee Setback, Pointe Coupee Parish, Louisiana Relocation of Facilities	26 Mar 76
39	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-90.5-L, Chalmette Slip Levee Enlargement, St. Bernard Parish, Louisiana Relocation of Facilities	5 May 77
40	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-95.5-L, Thalia-Poydras Levee Setback, Orleans Parish, Louisiana, Relocation of Facilities	10 Jun 76
41	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-217.5-L, Chariot Levee Setback, East Baton Rouge Parish, Louisiana, Relocation of Facilities	11 Jun 76
42	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-52.6-R and M-52.1-R Junior and Lower Junior Levee Setbacks, Plaquemines Parish, Louisiana, Relocation of Facilities	24 Jan 77

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
43	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-86.0-R, Stanton Levee Setback, Orleans Parish, Louisiana, Relocation of Facilities	10 Oct 78
44	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M- 215.3-L, Manchac Bend Levee Setback, East Baton Rouge Parish, Louisiana, Relocation of Facilities	23 Dec 76
45	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-28.5-R, Empire Levee Setback, Plaquemines Parish Louisiana, Relocation of Facilities	16 Dec 77
46	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-49.0-R, Woodland Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	28 Nov 77
47	Flood Control, Mississippi River and Tributaries, Atchafalaya Basin Floodway, New Berwick Floodwall, St. Mary Parish, Louisiana, Relocation of Facilities	14 Mar 79
48	Flood Control, Mississippi River and Tributaries, Atchafalaya Basin Floodway, New Morgan City Floodwall, St. Mary Parish, Louisiana, Relocation of Facilities	16 Mar 81
49	Flood Control, Mississippi River and Tributaries, Atchafalaya Basin Floodway, New Tiger Island Floodwall, St. Mary Parish, Louisiana, Relocation of Facilities	17 Oct 79

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
50	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-24.8-R, Buras-Triumph Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	30 Sep 77
51	Flood Control, Mississippi River and Tributaries, East Atchafalaya Basin Protection Levee, Item E-69.0 and E-.73.3, Levee Enlargements, St. Martin and Iberville Parishes, Louisiana, Relocation of Louisiana State Route 997	3 Jun 77
52	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-100.0-L, Nashville-Napoleon Floodwall, Orleans Parish, Louisiana Relocation of Facilities	25 Aug 77
53	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-88.0-R, Algiers Lock Forebay Levee Enlargement, Orleans Parish, Louisiana, Relocation of Facilities	1 Aug 77
54	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-29.4-R, Empire Lock Modifications, Plaquemines Parish, Louisiana, Relocation of Facilities	19 May 78
55	Flood Control, Mississippi River and Tributaries, West Atchafalaya Basin Protection Levee, Item W-86.0 Levee Enlargement, St. Mary Parish, Louisiana, Relocation of Facilities	NOT SCHEDULED
56	Flood Control, Mississippi River and Tributaries, East Atchafalaya Basin Protection Levee, Item E-89, Driving Steel Sheet Piling and Levee Enlargement, St. Mary Parish, Louisiana, Relocation of Facilities	15 May 78

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
57	Flood Control, Mississippi River and Tributaries, Algiers Point Setback, Levee Enlargement and Slope Pavement, Item M-94.3-R, Orleans Parish, Louisiana, Relocation of Facilities	8 Aug 79
58	Flood Control, Mississippi River and Tributaries, Item M-48.7-L, East Pointe-a-la-Hache Ferry Landing and Levee Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	2 Jun 78
59	Flood Control, Mississippi River and Tributaries, Jackson Avenue Floodgate, Item M-97.2-L, Orleans Parish, Louisiana, Relocation of Facilities	9 Jun 78
60	Flood Control, Mississippi River and Tributaries, Item M-303 to 293-R, Torras Levee Enlargement and Berm, Point Coupee Parish, Louisiana, Relocation of Facilities	26 Jul 78
61	Flood Control, Mississippi River and Tributaries, East Atchafalaya Basin Protection Levee, Item E-36.0, Levee Enlargement, Iberville Parish, Louisiana, Relocation of Facilities	28 Mar 79
62	Flood Control, Mississippi River and Tributaries, Item M-292 to 283-R, Raccourci Levee Enlargement, Pointe Coupee Parish, Louisiana, Relocation of Facilities	12 Jun 79
63	Flood Control, Mississippi River and Tributaries, West Atchafalaya Basin Protection Levee, Item W-85, Levee Enlargement, Iberia and St. Martin Parishes, Louisiana, Relocation of Facilities	6 Oct 80

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
64	Flood Control, Mississippi River and Tributaries, Louisiana-Jackson Avenue Floodwall, Item M-98.2 to 97.2-L, Orleans Parish, Louisiana, Relocation of Facilities	1 Jun 82
65	Flood Control, Mississippi River and Tributaries, Canal-Toulouse Street Floodwall, Item M-94.9-94.6-L, Orleans Parish, Louisiana, Relocation of Facilities	17 Dec 82
66	Flood Control, Mississippi River and Tributaries, West Atchafalaya Basin Protection Levee, Item W-117.0, Levee Enlargement, St. Mary Parish, Louisiana, Relocation of Facilities	15 Dec 80
67	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-56.1-L, Harlem Levee Setback Plaquemines Parish, Louisiana, Relocation of Facilities	17 Feb 82
68	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-71.0-L, Linwood Levee Setback, Plaquemines Parish, Louisiana, Relocation of Facilities	16 Mar 81
69	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-75.6 - 74.5-L, Scarsdale-Stella Levee Setback and Enlargement, Plaquemines Parish, Louisiana, Relocation of Facilities	9 Jun 81
70	Flood Control, Mississippi River and Tributaries, West Atchafalaya Basin Protection Levee, Item W-68.0, Levee Enlargement & Berms, St. Martin Parish, Louisiana, Relocation of Facilities	Jul 83

STATUS OF DESIGN MEMORANDUMS (cont'd)

<u>Design . Memo No.</u>	<u>Title</u>	<u>Submission Date</u>
71	Flood Control, Mississippi River and Tributaries, East Atchafalaya Basin Protection Levee, Item E-58.0A, Levee Enlargement & Berms, Iberville Parish, Louisiana, Relocation of Facilities	27 Mar 81
72	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-94.3-R and M-97.2-R, Algiers and Gretna Ferry Terminal Floodwalls, Orleans and Jefferson Parishes, Louisiana, Relocation of Facilities	2 Nov 81
73	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-94.3, Algiers Point Setback and Levee Enlargement, Orleans Parish, Louisiana, Relocation of Facilities	20 Jun 83
74	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-94.3-93.5-L, Barracks-Desire Street Floodwall, Orleans Parish, Louisiana, Relocation of Facilities	Apr 84
75	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-93.5-92.8-L, Desire-IHNC Floodwall, Orleans Parish, Louisiana, Relocation of Facilities	Apr 84
76	Flood Control, Mississippi River and Tributaries, Mississippi River Levees, Item M-90.5 - 89.5-L, Chalmette Battlefield, St. Bernard Parish, Louisiana, Relocation of Facilities	Jul 83

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES

RELOCATION DESIGN MEMORANDUM NO. 73

PERTINENT DATA

LOCATION OF PROJECT

The Algiers Point Setback is located on the westbank of a major bendway of the Mississippi River in New Orleans, Louisiana, approximately 94 river miles upstream from the Head of Passes, between right bank levee stations 49+00 and 71+50.

TYPE OF PROJECT

The proposed project consists of a full earthen MR&T levee with concrete slope pavement and ramp.

AUTHORITY

The Flood Control Act of 15 May 1928 (Public Law 391, 70th Congress), as amended, provides authorization for the project.

GENERAL DATA

	<u>Unit/Datum</u>	
Design levee grade	El; NGVD	23.5
Construction levee grade	El, NGVD	25.5
Freeboard	ft	4.5
Project design flow	c.f.s.	1,250,000
Length of levee	ft	1,790

AFFECTED FACILITIES

<u>OWNER</u>	<u>ITEM</u>	<u>LOCATION</u>
Board of Commissioners, Port of New Orleans	Fence, Paved ramp and Waterlines	Plate 4&5
City of New Orleans	Streets and Drains	Plate 3
City of New Orleans	Brake Tag Station	Plate 4
City of New Orleans	3rd District Landing	Plate 4
City of New Orleans	Sewer & Water Mains	Plate 5
Cox Cable, Inc.	TV Cable	Plate 6
Louisiana Power and Light Co.	Powerlines	Plate 6
South Central Bell Telephone, Co.	Telephone lines	Plate 6
New Orleans Public Service, Inc.	Gaslines	Plate 7

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES
WESTBANK ORLEANS PARISH
LOUISIANA

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT
RELOCATION OF FACILITIES
DESIGN MEMORANDUM NO. 73

PREPARED BY
TAHIR QIZILBASH AND RAYMOND E. ZIMNY
FOR THE
DEPARTMENT OF THE ARMY
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS
DESIGN SERVICES BRANCH
RELOCATIONS SECTION
NEW ORLEANS, LOUISIANA

JUNE 1983



1. PURPOSE. This design memorandum will present the plan for the relocation of facilities which will be affected by the construction of item M-94.3-R, Algiers Point Setback and Levee Enlargement. It is in compliance with applicable engineering regulations and establishes the necessity for the relocations described. It documents the legal obligations of the Federal Government as to compensation or reimbursement to owners and provides a basis for negotiations with the facilities owners and for preparation of plans and specifications.

2. PROJECT AUTHORIZATION

a. Setback Projects For The Main Line Mississippi Levees

(1) The Act of 17 May 1950, Public Law 516, 81st Congress, 2d Session, authorized that flood control improvements, substantially as contemplated by the Flood Control Act of 15 May 1928, Public Law 391, 70th Congress, as amended, be extended to include such improvements in the Parish of Orleans.

(2) Landward movement of the riverside toe of the levee to avoid attack by the river is the governing criterion in determining whether a project is a levee setback performed under the category of maintenance work.

b. Enlargement Projects For The Main Line Mississippi River
Levees

(1) The authority for construction, enlargement, or improvement of mainline levees is substantially the same law as above, except that the furnishing of right-of-way for levee foundations and levees is an obligation of local interests.

(2) Projects along the present alinement of flood control which involve flattening of levee slopes, raising of net levee grade, or the construction of berms are classified as enlargements and are funded under the category of construction.

c. Requirements of Local Cooperation

(1) The Acts cited above require that the levee board,

(a) maintain all flood control structures, except controlling and regulating spillway structures, including special relief levees; maintenance includes normally such matters as cutting grass, removal of weeds, local drainage and minor repairs of main river levees.

(b) accept all lands turned over to it under the provisions of Section 4 of the said Act of 15 May 1928.

(c) provide without cost to the United States, except as provided in Section 3(d) under item "Lower Mississippi River" of the Flood Control Act approved 18 August 1941, all rights of way for levee foundations and levees on the main stem of the Mississippi River and such drainage works as may be made necessary by levee construction.

(d) hold and save the United States free from damage claims resulting from the construction of the project.

(2) Section 3(d), cited above, of the Flood Control Act of 18 August 1941, Public Law 228, 77th Congress, 1st Session, Section 3, under item "Lower Mississippi River" authorized the Chief of Engineers, with the approval of the Secretary of the Army, to reimburse local authorities for actual reasonable expenditures for providing, in accordance with local legal procedure or custom, rights of way and flowage easements required for future setbacks of main line Mississippi River Levees.

(3) The required local assurances for this project from the Board of Commissioners of the Orleans Levee District (Levee Board), were expanded to provide that they will comply with the provisions of the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, Public Law 91-646.

(4) Local interests are organized into a legally constituted levee district and have given satisfactory assurances of local cooperation for all authorized work and have agreed to perform and execute the obligations and requirements of local cooperation as set forth in the 15 May 1928 Flood Control Act. The Board of Commissioners of the Orleans Levee District furnished assurances of local cooperation and supporting resolution of this project on 19 September 1950. Supplemental assurances covering the provisions of Public Law 91-646 were executed by the Levee Board on 1 February 1973 and approved on 13 April 1973.

3. AUTHORITY FOR ACCOMPLISHING RELOCATIONS

a. Public Law (PL)

(1) Under Special Statutory Authority, "Payment for structures owned by an agency of government may be provided, whenever, during the construction or reconstruction of any navigation, flood control, or related water development project under the direction of the Secretary of the Army, the Chief of Engineers determines that any structure or facility owned by an agency of government and utilized in the performance of a governmental function should be protected, altered, reconstructed, relocated, or replaced to meet the requirements of navigation or flood control, or both; or to preserve the safety or integrity of

such facility when its safety or usefulness is determined by the Chief of Engineers to be adversely affected or threatened by the project, the Chief of Engineers may, if he deems such action to be in the public interest, enter into a contract providing for the payment from appropriations made for the construction of maintenance of such project, of the reasonable actual cost of such remedial work, or for the payment of a lump sum representing the estimated reasonable cost: Provided, that this section shall not be construed as modifying any existing or future requirement of local cooperation, or as indicating a policy that local interests shall not hereafter be required to assume costs of modifying such facilities. The provisions of this section may be applied to projects hereafter authorized and to those authorized but not completed as of the date of this Act, and notwithstanding the navigation servitude vested in the United States, they may be applied to such structures or facilities occupying the beds of navigable waters of the United States." (Section 111.)

(2) Payment or reimbursement of cost to local cooperating agencies for rights-of-way and flowage easements required for setbacks including necessary relocation or alteration of public roads, highways, railroads, public utilities and pipelines is authorized by Section 3(d) of the Flood Control Act of 18 August 1941, Public Law 228, 77th Congress 1st Session as amended by Flood Control Act of 22 December 1944, Public Law 534 78th Congress 2d Session.

(3) Reimbursement of costs to local cooperating agencies in perfecting relocation of interfering facilities may be made pursuant to the Act of 23 April 1934, Public Law 171, 73rd Congress. In those cases where the local cooperating agency is not financially able to arrange for relocation or alteration of interfering facilities, subject to reimbursement of costs, the District Engineer is authorized to enter into a contract with the facility owner, subject to requirements of engineering regulations.

b. Attorney's Report on Compensable Interest. The legal obligations of the Federal Government to the owners of affected facilities and the extent of authority for the relocations are as established in the Attorney's report which is attached as Appendix I. Please note that the term existing levee rights-of-way refers to the levee right-of-way acquired prior to 1976. No relocations have been effected subsequent to that acquisition.

(1) Roads. The attorney's report provides that the City of New Orleans Department of Streets has a compensable interest in their affected facilities when facilities are serving a valid governmental function. Applicable law authorizes relocation when such facilities continue to serve in their original capacity, wherever they are located. Therefore, the cost of relocating items R-1, R-2, R-3, and R-4 will be borne by the Federal Government.

(2) Municipal Structures. The attorney's report provides that the City of New Orleans, Department of Property Management has a compensable interest in their affected facilities, since there is a continuing requirement for the use of these structures in the performance of a governmental function. Therefore, the cost of relocating items B-1 and L-1 will be borne by the Federal Government.

(3) Utilities

(a) Sewerage and Water Board of New Orleans (S&WB). The attorney's report states that the S&WB has a compensable interest in all of its facilities affected by the project. Therefore, the relocations of items W-1, W-2, S-1, and S-2 will be accomplished at Federal expense.

(b) Board of Commissioners of the Port of New Orleans, (Dock Board). The attorney's report finds that the Dock Board, an agency of the State of Louisiana, has a compensable interest in all of its facilities that will be affected by the project. Therefore, the relocations of items W-3, W-4, F-1, and R-5 will be accomplished at Federal expense.

(c) South Central Bell Telephone Company (SCB), Louisiana Power and Light Company (LP&L), and New Orleans Public Service, Inc. (NOPSI). The attorney's report finds that, SCB, LP&L, and NOPSI are privately owned public utilities which have a compensable interest in their facilities affected by the project, excepting any facility or portion of that facility which is located within the existing levee right-of-way. In accordance with the attorney's report, the cost of relocating the facilities which are located outside of the existing levee right-of-way will be borne by the Federal Government. The relocations of the facilities located within the existing levee right-of-way will be accomplished at the individual owner's expense.

(d) Cox Cable of New Orleans, Inc. (Cox). The attorney's report finds that Cox Cable is not considered to be either a public utility or common carrier, and the cable company does not have a compensable interest in any of their facilities which are located within the existing levee right-of-way. The portions of their facilities located within the new levee right-of-way will be relocated as part of the real estate acquisition process.

c. References. To facilitate review of this memorandum the following references are listed:

(1) Post-Authorization Studies Report, Item M-94.3-R Algiers Point Setback, Phase 2, STATION 49+00 TO 71+50 Orleans Parish, Louisiana, approved 25 JAN 1983.

(2) Relocation Design Memorandum No. 57, Algiers Point Setback, Levee Enlargement and Slope Pavement, Orleans Parish, Louisiana, approved 12 Sep 1979.

(3) DIVR 1110-2-1, ENGINEERING AND DESIGN, Relocation of Facilities - MR&T, 29 Jan 1968.

(4) ER 1110-2-1150, ENGINEERING AND DESIGN, POST-Authorization Studies, 1 Oct 1971.

(5) ER 110-2-1, ENGINEERING AND DESIGN, Planning, Engineering, and Design Services, 12 Feb 1973.

(6) ER 1180-1-1, Engineer Contract Instruction, ECI, 1 Jul 1980.

(7) LMVD Supplement 1 to ER 1110-2-1150, 1 Oct 1981.

4. DESCRIPTION OF AFFECTED FACILITIES

a. Setback Limits

(1) Roads.

(a) The City of New Orleans, Department of Streets (Streets Dept.) owns item R-1 (Seguin Street, Station 51+26, plate 3), item R-2 (Bermuda Street, Station 55+13, plate 3), item R-3 (Patterson Street, Station 54+93 to Station 59+57), plate 3) and item R-4 (Morgan Street, Station 51+11 to Station 59+29, plate 3). Each of the streets listed above, with the exception of Morgan Street, is a 20-foot-wide, asphalt-paved, two-lane roadway. Approximately 120 linear feet of item R-1, 120 linear feet of item R-2, and 500 linear feet of item R-3 will be affected by the project. Items R-1, R-2, and R-3 are in poor to fair condition with numerous potholes evident.

(b) Item R-4, Morgan Street is a 34-foot wide asphalt-paved roadway. This facility consists of two traffic lanes, one parking lane, side walk area and curb and gutter section. The sidewalk, curb, and gutter are constructed of various materials with brick, stone block, and concrete intermixed. Approximately 700 linear feet (north lane only, see plates 3 and 10) of this facility will be affected by the project. Item R-4 is in fair condition.

(c) The Dock Board owns item R-5 which is located at approximate station 59+70 (plate 4). Item R-5 represents a 20-foot wide asphalt-paved ramp. Approximately 160 linear feet of this facility will be affected by the project. Item R-5 is in good condition.

(2) Municipal Structures. The City of New Orleans, Department of Property Management (Property Management Dept.) owns item B-1 (Brake Tag Station) which is located at the intersection of Patterson Street and Bermuda Street (plate 4). One lane of Patterson Street (approximately 290 feet in length) is used as a stacking lane for motor vehicle inspection. The Brake Tag Station is a 179' x 60' one story metal building on a concrete slab foundation. Only 153' x 60' is used for the safety inspection of motor vehicles. This portion contains an office, restroom, and miscellaneous equipment used for motor vehicle inspection. This facility shares a common wood frame interior wall with a 26' x 60' area used daily by the City for a Council on Aged meeting hall. Item B-1 is in fair condition.

(3) Utilities

(a) Drainage Facilities. The City of New Orleans, Department of Streets (Streets Dept.) owns the following drainage facilities. It is assumed that these items are in good condition since they are functioning satisfactorily.

i. Item D-1. This item is a 15-inch reinforced concrete pipe (R.C.P.) storm drain along the north side of Morgan Street (station 51+24, plate 3). This pipe connects to an existing storm drain system which crosses under Morgan Street and runs southerly along Seguin Street. Approximately 28 linear feet of pipe will be affected by the project.

ii. Item D-2. This item is a curb inlet-type catch basin (station 51+38, plate 3).

iii. Item D-3. This item consists of 2 inlets with grated covers located at north corner of the intersection of Morgan Street and Bermuda Street (Station 54+88, plate 3). These inlets are connected to pipes which cross Morgan Street and run southerly along Bermuda Street.

(b) Waterlines. The Sewerage and Water Board of New Orleans (S&WB) owns items W-1 and W-2, and Board of Commissioners of the Port of New Orleans (Dock Board) owns items W-3 and W-4. These facilities are assumed to be in good condition as they are functioning satisfactorily.

i. Item W-1. This item is a 6-inch diameter cast iron potable waterline located under Seguin Street (Station 51+26, plate 5). Approximately 110 linear feet of this facility will be affected by the project. This facility was installed in 1904 and has a design life of 100 to 150 years.

ii. Item W-2. This item is a 6-inch diameter cast iron potable waterline located under Patterson Street from Station 55+13 to Station 59+57, (plate 5). Approximately 500 linear feet of this facility (including one fire hydrant and one water meter) will be affected by the project. This facility was installed in 1904 and has a design life of 100 to 150 years.

iii. Item W-3. This item is a 4-inch diameter cast iron waterline located at approximate station 56+00 (plate 5). This item serviced the Bermuda Street Wharf, but has been taken out of service. Approximately 30 linear feet of this waterline will be affected by the project.

iv. Item W-4. This item is a 6-inch diameter cast iron waterline located at approximate station 60+10 (plate 5). This waterline is out of service. Approximately 130 linear feet of this facility will be affected by the project. Item W-4 was installed in 1968, and it has a design life of 75 years.

(c) Sanitary Sewers. The Sewerage and Water Board of New Orleans (S&WB) owns the following sewer facilities.

i. Item S-1. This item (plate 5) consists of an 8-inch diameter sewerline and three manholes located under Patterson Street between stations 51+90 and 58+96. Approximately 750 linear feet of this facility (including manholes) will be affected by the project. Item S-1 was installed in 1940 and has a design life of 100 to 150 years. Item S-1 is assumed to be in good condition.

ii. Item S-2. This item (plate 5) consists of an 8-inch diameter sewerline located under Bermuda Street at approximate station 55+13. Approximately 130 linear feet of this facility will be affected by the project. Item S-2 was installed in 1940 and has a design life of 100 to 150 years. Item S-2 is assumed to be in good condition.

(d) Gaslines. The New Orleans Public Service, Inc., (NOPSI) owns the following gas facilities.

i. Item G-1. This item is a 3-inch diameter high pressure gasline located under Bermuda Street at approximate station 55+13 (plate 7). Approximately 160 linear feet of this facility will be affected by the project. Item G-1 was installed in 1928 and is in good state of maintenance.

ii. Item G-2. This item is a 1-inch diameter gasline located under Patterson Street between approximate stations 53+56 and 56+65 (plate 7). Approximately 330 linear feet of this facility (including one meter) will be affected by the project. Item G-2 was installed in three segments. In 1943 - 80 linear feet, in 1945 - 90 linear feet, and in 1958 - 165 linear feet was installed. Item G-2 is in good state of maintenance.

(e) Powerlines. The Louisiana Power and Light Company (LP&L) owns various facilities within the setback limits which will be affected by the project. The installation date of LP&L facilities varies from 1962 to 1973 and design life is 30 years. The facilities are in good condition and are as follows:

i. Item P-1. This item is a 13.8kV aerial powerline located between stations 51+06 and 57+92 (plate 6). Approximately 1,100 linear feet of this facility will be affected by the project. This facility services the Dock Board's fireboat dock on the river.

ii. Item P-2. This item is an aerial powerline located at station 55+00 (plate 6). Approximately 40 linear feet of this facility will be affected by the project. This facility services the City Brake Tag Station.

iii. Item P-3. This item is a 13.8 kV aerial powerline which crosses the levee at approximate station 60+40 and services the Bermuda Street Wharf on the river (plate 6). Approximately 130 linear feet of this facility will be affected by the project.

(f) Telephone Facilities. South Central Bell Telephone Company (SCB) owns the following facilities within the setback limits which will be affected by the project.

i. Item T-1. This item is an aerial telephone cable which runs along Morgan Street and Seguin Street between stations 50+96 and 58+20 (plate 6). This facility services the Dock Board's fireboat dock on the river. Approximately 900 linear feet of this facility will be affected by the project. Item T-1 was installed in 1976 and has a design life of 35 years. This facility is in good condition.

ii. Item T-2. This item is an aerial telephone cable which runs on LP&L poles along Patterson Street between stations 55+00 and 60+40 (plate 6). This facility services the City's Brake Tag Station and Bermuda Street Wharf. Approximately 480 linear feet of this facility will be affected by the project. Item T-2 was installed in 1976 and has a design life of 35 years. This facility is in good condition.

(g) Cable Television. Cox Cable of New Orleans, Inc. (Cox) owns the following facilities within the setback limits which will be effected by the project.

i. Item TV-1. This item is an aerial television cable which runs on SCB poles along Morgan Street between stations 50+96 and 58+20 (plate 6). Approximately 600 linear feet of this facility will be affected by the project. Item TV-1 was installed in 1983 and has a design life of 15 years. This facility is in excellent condition.

ii. Item TV-2. This item is an aerial television cable which runs on LP&L poles along Patterson Street between stations 56+53 and 60+40 (plate 6). Approximately 300 linear feet of this facility will be affected by the project. Item TV-1 was installed in 1983 and has a design life of 15 years. This facility is in excellent condition.

(h) Miscellaneous. Item F-1 (plate 4) represents a 6-foot high chainlink fence located along Patterson Street between station 55+28 and 60+80. This facility is owned by the Dock Board. Approximately 700 linear feet of this facility will be affected by the project. This facility is in good condition.

b. Enlargement Limits.

(1) Municipal Structures. The City of New Orleans, Department of Property Management (Property Management Dept.) owns item L-1 (3rd Dist. Ferry Landing) which is located adjacent to the landside levee toe at station 66+15 (plate 3). This facility consists of a reinforced concrete retaining wall, concrete steps and vehicle ramps. The wall is 280 linear feet in length and varies in height from 1 to 10 feet. Item L-1 will be affected by the project. This facility is currently not in use as a ferry landing. The ramps and steps are used by pedestrians and cyclists as access to the levee crown for recreation.

(2) Utilities.

Powerlines. The Louisiana Power and Light Company (LP&L) owns item P-4 (plate 6) which is located within the enlargement limits and which will be affected by the project. Item P-4 is a 13.8 kV aerial powerline which crosses the levee at station 64+10. Approximately 130 linear feet of this facility will be affected by the project. Item P-4 is in good condition.

5. FIELD RECONNAISSANCE AND INVESTIGATION. Field surveys, field inspections, consultations, and correspondence with the affected facility owners provided the basic information from which this design memorandum and the proposed relocations plans were prepared. This work was accomplished principally by Messrs. Tahir H. Qizilbash and Raymond E. Zimny of this District's Relocations Section, Design Services Branch, Engineering Division.

6. CRITERIA FOR RELOCATED FACILITIES.

a. Roads.

(1) The design criteria for the relocation of items R-1, R-2, R-3, and R-4 (plates 3 and 4) will be in accordance with the provisions of the "General Specifications and Standard Plans for Street Paving and Temporary Surfacing of the City of New Orleans, Louisiana", adopted 28 November 1969. The design standards used for the proposed streets relocations are the design standards used by the City of New Orleans Department of Streets and, therefore, do not result in a betterment. The streets alterations will be designed by this District, and the construction will be performed by the Government contractor.

(2) No design criteria is required for Dock Board facility item R-5 (plate 4), since this will be removed by the Government contractor.

b. Municipal Structures. The design and relocation of the Brake Tag Station (item B-1, plate 4) will be in accordance with Specifications and Standard Plans for Motor Vehicle Inspection Stations, as provided by the Safety and Permits Department of the City of New Orleans. The new building will be sized to handle the same vehicle capacity as existing facility, unless the City wishes to pay for a betterment. No design criteria are required for City's 3rd District Ferry Landing (item L-1, plate 4), since item L-1 will be removed by the Government contractor.

c. Utilities.

(1) The owners of public utilities (S&WB, NOPSI, LP&L, and SCB) will accomplish the design and relocation of their facilities to their criteria, and in a manner which will eliminate interference with the project. This District will review the construction drawings for the relocations to insure that no betterments are included at Federal expense. During construction and upon completion of construction, this District will inspect the relocations in the field to insure conformance with project requirements.

(2) No design criteria are required for Dock Board facilities, item W-3, W-4, and F-1 (plates 4 and 5), since they will be removed by the Government contractor.

(3) Cox Cable of New Orleans, Inc. will adjust or remove their facilities (items TV-1 and TV-2, plate 6) to their individual criteria and in a manner which will eliminate interference with the project. This District will inspect the relocations to insure conformance with project requirements. These facilities will be relocated as part of the real estate acquisition program.

7. DESCRIPTION OF PROPOSED RELOCATIONS

a. Setback Limits.

(1) Roads.

(a) The existing items R-1, R-2, and R-3 (Seguin Street, Bermuda Street, and Patterson Street) will be abandoned by the City of New Orleans, Department of Streets. The Government contractor will remove the existing road surfaces of item R-1, R-2, and R-3 and widen Morgan Street (item R-3) 6 feet on one side at the location and according to the details shown on plate 10. Item R-4 will be constructed due to closing of Patterson Street. The existing Patterson Street corridor provides parking and stacking lanes for Algiers Ferry traffic during rush hours. Both Patterson Street and Morgan Street lead to Algiers Ferry

Terminal. The additional lane along Morgan Street will allow replacement of traffic function of Patterson Street. Removal of items R-1, R-2, R-3, and construction of item R-4 will be at the expense of the Federal Government.

(b) The Dock Board will abandon item R-5. Item R-5 will be removed by the Government contractor at the expense of the Federal Government. The Dock Board does not require the replacement of this facility.

(2) Municipal Structures. The existing item B-1 (Brake Tag Station) will be removed by the Government contractor. The City of New Orleans will design and construct the new brake tag station in accordance with their specifications and standards. Item B-1 will be relocated by the City of New Orleans at the expense of the Federal Government.

(3) Utilities.

(a) Drainage Facilities. The existing city's drainage facilities items D-1, D-2, and D-3 will be removed and reconstructed in conjunction with the widening of Morgan Street (item R-3) by the Government contractor at the expense of the Federal Government.

(b) Waterlines.

i. Items W-1 and W-2 (plate 5). The existing S&WB waterlines will be removed and relocated by the Government contractor at the expense of the Federal Government. The affected segments of item W-1 (under Seguin Street) will be removed. Item W-1 will be relocated at the same alinement directly over the new levee. Item W-2 will be relocated at Bermuda Street alinement directly over the new levee.

ii. Items W-3 and W-4 (plate 5). The Dock Board will abandon items W-3 and W-4. These facilities will be removed by the Government contractor at the expense of the Federal Government. The Dock Board does not require the replacement of these facilities.

(c) Sanitary Sewers. The S&WB will abandon the item S-1 which is under Patterson Street. The affected segment of Item S-1 (plate 5) will be removed by Government contractor. Item S-2 which is under Bermuda Street (plate 5) will require relocation at the same alinement. Presently item S-2 is a gravity sewerline. It will be replaced as a force main and will be installed over the new levee. Two manholes will be constructed at the landside toe of the levee. The replacement of item S-2 is

required by the Dock Board to serve the Bermuda Street Wharf. The Dock Board will assure that the present pump is adequate for the force main. The work will be accomplished by the Government contractor at the expense of the Federal Government.

(d) Gaslines.

i. Item G-1 (plate 7) which is under Bermuda Street will be relocated by NOPSI in a manner that will not interfere with the levee work. Item G-1 is located outside the existing levee right-of-way, therefore relocation will be performed at the Federal Government's expense.

ii. Item G-2 (plate 7) which is under Patterson Street will be removed by NOPSI at the expense of the Federal Government since it is located outside the existing levee right-of-way. Item G-2 will not be replaced.

(e) Powerlines.

i. Item P-1 (plate 6). The interfering 13.8kV aerial powerline and aerial guy wires located at Patterson Street and Seguin Street will be relocated by LP&L in a manner that will not interfere with the project. Item P-1 is located outside the existing levee right-of-way and will be relocated at Federal expense.

ii. Item P-2 (plate 6). LP&L will remove the aerial powerline which services the Brake Tag Station. The Federal Government will pay only for portion of the item P-2 which require removal as a result of the levee project.

iii. Item P-3 (plate 6). LP&L will remove this 13.8kV aerial powerline which services the Bermuda Street Wharf. Item P-3 is located outside the existing levee right-of-way. The Federal Government will pay for the removal of this facility. Bermuda Street Wharf is getting power service from other line (item P-1); therefore, the replacement of item P-3 is not required.

(f) Telephone Facilities.

i. Item T-1 (plate 6). The interfering aerial telephone line located at Morgan Street and Seguin Street will be relocated by SCB in a manner that will not interfere with the project. It will be relocated from the northside of Morgan Street to the southside of Morgan Street and will cross the new levee on LP&L poles. Item T-1 is located outside the existing levee right-of-way and will be relocated at Federal expense.

ii. Item T-2 (plate 6). SCB will remove item T-2 located on Patterson Street which services the Bermuda Street Wharf and Brake Tag Station. The Federal Government will pay for the removal of item T-2, since it is located the outside the existing levee right-of-way. The Bermuda Street Wharf will get telephone service from SCB line coming from Seguin Street.

(g) Cable Television.

i. Item TV-1 (plate 6). The interfering TV cable located on SCB poles along northside of Morgan Street will be relocated by Cox Cable to the existing LP&L poles on the southside of Morgan Street. Item TV-1 is located outside the existing levee right-of-way. It will be relocated by Cox Cable at the expense of the Federal Government through real estate acquisition.

ii. Item TV-2 (plate 6). This TV cable located on LP&L poles at Patterson Street will be removed from Patterson Street and will be relocated along Bermuda Street by Cox Cable. The Federal Government, by real estate acquisition program, will reimburse the Cox Cable for the relocation of item TV-2.

iii. Miscellaneous. Item F-1 (plate 4) is a chainlink fence owned by the Dock Board. This facility will be removed by Government contractor at the expense of the Federal Government.

b. Enlargement Limits.

(1) Municipal Structures. The City of New Orleans may abandon item L-1 (3rd Dist Ferry Landing) in that case the Government contractor will remove the facility at the expense of the Federal government.

(2) Utilities.

Powerline. Item P-4 (plate 6) is a 13.8 kV aerial powerline. This facility will be removed by LP&L. The Federal Government will pay that portion of removal cost which is within the new levee right-of-way.

8. PROCEDURE FOR ACCOMPLISHING RELOCATIONS.

a. Roads. It is proposed that the preparation of detailed plans and specifications for the reconstruction of affected streets be accomplished by the Federal Government in conjunction with the plans and specifications for the new levee. Under this proposal, the reconstruction of Morgan Street would be carried out with the construction of the levee through a competitively bid contract. The right-of-way required for item R-4 is shown on plate 3.

b. Municipal Structure.

(1) The City of New Orleans will prepare the detailed plans and specifications for the new Brake Tag Station and the construction will be carried out by the City; at the expense of the Federal government. The removal of existing Brake Tag Station will be done by Government contractor. For reimbursement procedure to be used refer to applicable portions of following subparagraph c.(2).

(2) The City's 3rd District Ferry Landing may be removed by Government contractor.

c. Utilities.

(1) For all City owned utilities, it is proposed that preparation of detailed plans and specifications and reconstruction of affected drainage facilities, waterlines, and sewerlines be accomplished by the Federal Government in conjunction with design work for the new levee. Under this proposal, the relocation of City owned utilities would be carried out with the construction of the levee through a competitively bid contract.

(2) For all other utilities, the affected utility owners will prepare detailed plans and specifications for the cutting, abandoning, removing, or relocating of their facilities, and they will accomplish their work in a manner that will not interfere with the project. The owner's plans and specifications will be reviewed by this District to determine if the owners' proposals are compatible with the project. The relocations will be closely coordinated among all parties involved. The Levee Board, as the local assuring agency for the work, will advise the affected owners to effect their relocations. If the owners' work is to be performed at Federal expense, this district will advise the Levee Board of the dollar reimbursement to the owner. Payments will be based on actual cost for accomplishing the work, adjusted where appropriate, for depreciation, salvage value, and betterments.

Upon completion of the relocations for which reimbursements are to be made, the Levee Board will furnish New Orleans District Real Estate Division with itemized billings which should include a copy of any contracts (including plans and specifications) covering work performed by others or itemized billings for labor and materials if the work was performed by the owner, and a copy of all cancelled checks covering related payments, prior to the Levee Board's payment to the contractor and/or facility owners, etc., for New Orleans District Engineering Division's review and approval. Further, all records related to the reimbursement request will be subject to audit by the Government and original time cards of payrolls, material records, and accounts for all

charges and expenditures for which reimbursement will be claimed from the Government will be available at all reasonable times for Government inspection. Finally, as far as practicable, separate records will be maintained for all items and accounts constituting the basis of information from which the invoices are prepared. Variances between billings and the dollar limits available shall be resolved before payments are made. Upon New Orleans District Engineering Division's review and approval of the itemized billings submitted, New Orleans District Real Estate Division will instruct the Levee Board to make payment. After payments are made the Levee Board will furnish New Orleans District Real Estate Division with a reimbursement assembly consisting of a resolution of the Levee Board releasing the United States of America from all liabilities and claims, a billing, and a copy of the cancelled check with indorsement or receipt for check. Upon receipt and approval of the appropriate reimbursement assembly, reimbursement will be made.

(3) Unknown Owners. The Levee Board will declare as abandoned any interfering facilities for which owners are unknown. The Levee Board will also grant this District written releases to allow the Government contractor to remove such affected facilities.

9. ATTITUDE OF OWNERS.

a. Representatives of all affected facilities have indicated concurrence in the relocation plans and in the determination of compensable interest presented in this design memorandum and have expressed a willingness to cooperate in matters pertaining to their facilities. They are all aware that reimbursement for these relocations will be in accordance with the approved relocation design memorandum. It is noted that several meetings have been held with City officials to discuss the project, and obtain their approval in all matters relating to City facilities. Coordination with representatives of all concerned, indicates that there should be no serious problems regarding relocation of facilities. An unresolved issue is the relocation plan of the Brake Tag Station. While the City has indicated a willingness to relocate the brake tag operation, the specifics of the relocation are undefined at this time. The City has indicated that finding a site for the new Brake Tag Station which would be acceptable to all parties may be difficult. Also, in order to design and construct the new building the City may take 2 to 3 years. The City has not proposed any relocation plan for temporary continuation of service of the Brake Tag Station until a new facility is built.

b. The Chief of Planning for the Dock Board has actively participated in assisting this District in bringing the representatives of all facilities together at one time to iron out their relocation plans. These utilities will provide water, sewer, telephone and electric services to the Dock Board's fireboats as well as a commercial restaurant, scheduled to open August 1983 at Bermuda Street Wharf.

10. ESTIMATE OF COST. The estimated Federal cost for accomplishing the proposed relocations has been prepared in accordance with the provisions of ER-1180-1-1 and is based on plans proposed by or agreed to by the affected owners. The relocation costs are summarized in Table 1 below, and they are presented in more detail in tables 3 through 10 (Appendix II) of this report.

TABLE 1
SUMMARY OF ESTIMATED FEDERAL RELOCATIONS COST
(OCTOBER 1982 PRICE LEVELS)

Description/Owner	Const Funds (\$)	Maint Funds (\$)	Total Costs (\$)
Roads:	0	136,000	136,000
City of New Orleans			
Seguin St., Bermuda St., & Patterson Dr.	(0)	(37,000)	(37,000)
Morgan Street	(0)	(45,000)	(45,000)
Road R/W	(0)	(46,000)	(46,000) ^{3/}
Dock Board Ramp	(0)	(8,000)	(8,000)
Utilities:	10,000	310,000	320,000
Streets Dept (drainage)	(0)	(5,000)	(5,000)
S&WB	(0)	(61,000)	(61,000)
N.O. Property Management	(10,000)	(200,000)	(210,000)
Dock Board	(0)	(2,000)	(2,000)
LP&L	(0)	(16,000)	(16,000)
SCB	(0)	(15,000)	(15,000)
NOPSI	(0)	(11,000)	(11,000)
Subtotal	10,000	446,000	456,000 ^{1/}
E&D (6%+)	1,000	26,000	27,000 ^{2/}
S&A (6+)	<u>1,000</u>	<u>26,000</u>	<u>27,000^{2/}</u>
Total	12,000	498,000	510,000

^{1/} Includes Contingencies

^{2/} No E&D or S&A for Road R/W

^{3/} See Table 3

11. COMPARISON WITH PRIOR COST ESTIMATES. The current working cost estimates for the relocations required by Algiers Point project are summarized below for comparison with the latest approved cost estimates. The latest approved cost estimates have been separated from the respective documents for the Mississippi River Levees project for suitable comparison.

(a) Construction Funds. The latest approved cost estimate for construction funds is the Project Cost Estimate (PB-3), effective 1 October 1982, which was approved 29 September 1982.

TABLE 2A

COMPARISON OF COST - CONSTRUCTION FUNDS
(OCTOBER 1982 PRICE LEVELS)

<u>Item</u>	<u>Latest PB-3 (\$1,000's)</u>	<u>Current Working Estimate (\$1,000's)</u>	<u>Difference from PB-3 (\$1,000's)</u>
Relocations			
Roads	0	0	0
Utilities	0	10.0	+10.0
E&D	-	-	-
S&A	-	-	-
TOTAL	0	10.0	+10.0

(b) Maintenance Funds. The latest approved cost estimate for maintenance funds is the Detailed Project Schedule (PB-2A), CYP ES 013, effective 1 October 1982.

TABLE 2B

COMPARISON OF COST - MAINTENANCE FUNDS
(OCTOBER 1982 PRICE LEVELS)

<u>Item</u>	<u>Latest PB-2A (\$1,000's)</u>	<u>Current Working Estimate (\$1,000's)</u>	<u>Difference From PB-2A (\$1,000's)</u>
Relocations			
Roads	355.0	136.0	-219.0
Utilities	295.0	310.0	+15.0
E&D	-	-	-
S&A	-	-	-
TOTAL	650.0	446.0	-204.0

TABLE 2C
COMPARISON OF COST
CONSTRUCTION AND MAINTENANCE FUNDS
(OCTOBER 1982 PRICE LEVELS)

<u>Item</u>	Latest Costs (\$1,000's)	Current Working Estimate (\$1,000's)	Difference Approved Cost (\$1,000's)
Relocations			
Roads	355.0	136.0	-219.0
Utilities	295.0	320.0	+25.0
E&D	-	-	- *
S&A	-	-	- *
TOTAL	650.0	456.0	-194.0

*No comparisons are made for E&D and S&A. In the PB-3,2A tabulations, relocation feature E&D and S&A are not itemized. Any differences that could have been indicated are directly related to the particular relocation items, and therefore, would have the same justification for cost revision.

12. JUSTIFICATION FOR COST REVISION. Since the approved 1976 levee alignment for the Algiers Point project was not safe with the changed river conditions, this reach of the project was withdrawn from construction. The depth of the levee setback limit then was much less than now required. The latest PB-2A contained funds for road and utilities relocations as would have been necessitated by a 200-foot deep setback over an approximate length of 1,170 feet. A setback of those proportions would have allowed the relocation of Patterson Street as a two-lane roadway (approximate \$355,000) immediately adjacent to the landside levee toe with the relocation of attendant utilities (LPL, SCB, S&WB, Dock Board and NOPSI; approximate \$295,000); it would not have dislocated the Brake Tag Station. The setback presently proposed will toe out so close to Morgan Street that the road and utilities relocations will be reduced significantly. As currently

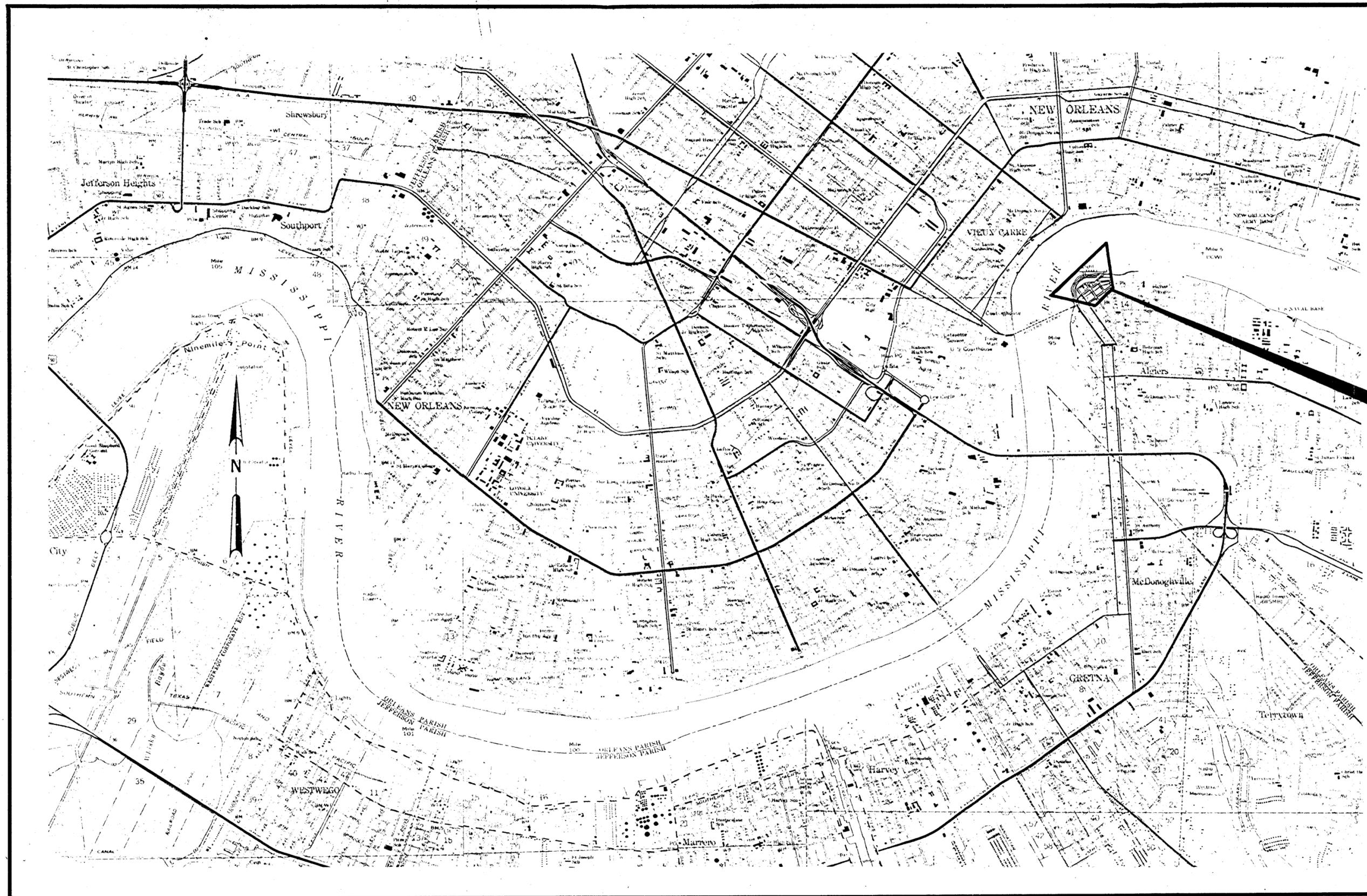
envisioned the addition to Morgan Street will cost \$91,000 and removal of Seguin, Bermuda, Patterson Streets and Dock Board ramp will cost \$45,000 for a reduction of \$219,000; the required utilities adjustment will cost \$110,000 for a reduction of \$185,00; the required Brake Tag Station adjustment will cost \$200,000; the required 3rd District Ferry Landing removal (affected by enlargement limit) will cost \$10,000. These changes in cost combine to provide a net decrease of \$194,000 as indicated in Table 2c.

13. RECOMMENDATIONS.

a. It is recommended that this design memorandum be approved as the basis for reimbursing affected owners for costs incurred in accomplishing the relocations described herein.

b. It is recommended that if the proposed site for the Algiers Brake Tag Station is not owned by the City of New Orleans, and cannot be obtained by donation or otherwise without cost then a letter supplement will be forwarded for transmittal to the Chief of Engineers pursuant to ECI 73-703.1(h).

c. It is recommended that if the City of New Orleans does not elect to take payment or partial payment from the Federal Government for the actual cost of a new site, that (section 73-703.1(j)) conveyance of title in the old site (i.e. land on which station now exists) to the United States will not be required. See Appendix III.

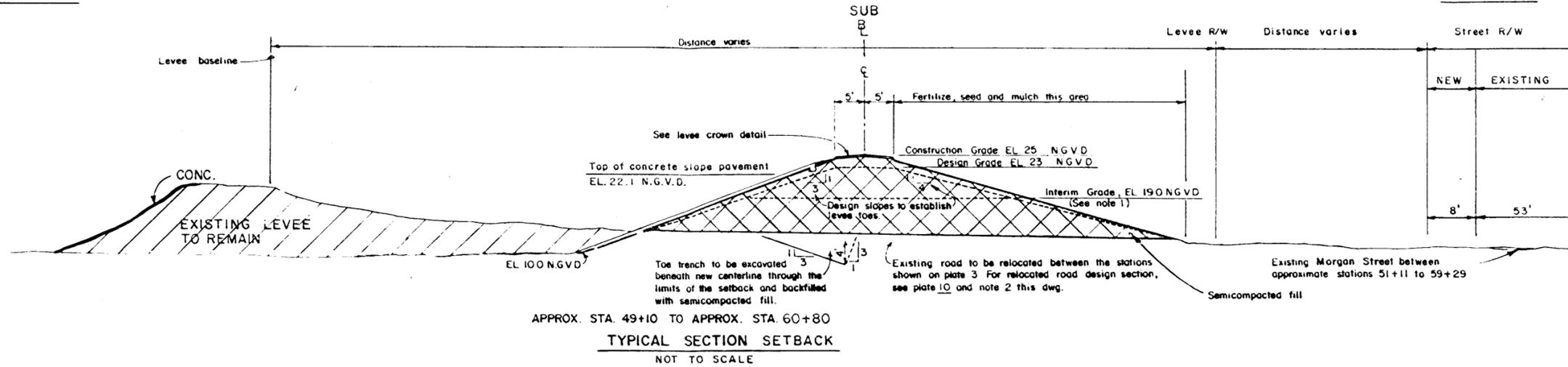


PLATES 2 THRU 10

MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
 ITEM M-94.3-R
 ALGIERS POINT SETBACK, LEVEE
 ENLARGEMENT AND SLOPE PAVEMENT
 DESIGN MEMORANDUM NO.73
PROJECT MAP
 CORPS OF ENGINEERS
 U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
 DATE: JUNE 1983 FILE NO. H-2-29567

RIVERSIDE

LANDSIDE

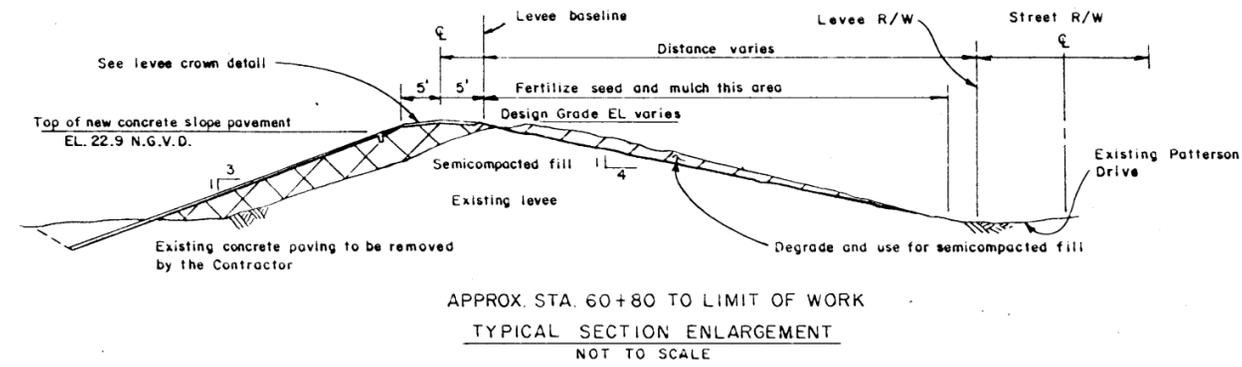


- NOTES FOR TYPICAL SECTION
- 1 The Contractor shall construct the setback levee to the interim grade of EL 19.0 N.G.V.D. with material hauled from the borrow pit as the initial method of construction of the setback
 - 2 The existing road underneath the setback levee shall be removed by excavating the road material down to a depth of 2' then backfilling with semicompacted levee embankment material
 - 3 There are abandoned foundations located within the limits of this typical section. The Contractor shall remove the foundations which are located within the new levee in their entirety

NOTE:
 See plate 10 for road detail

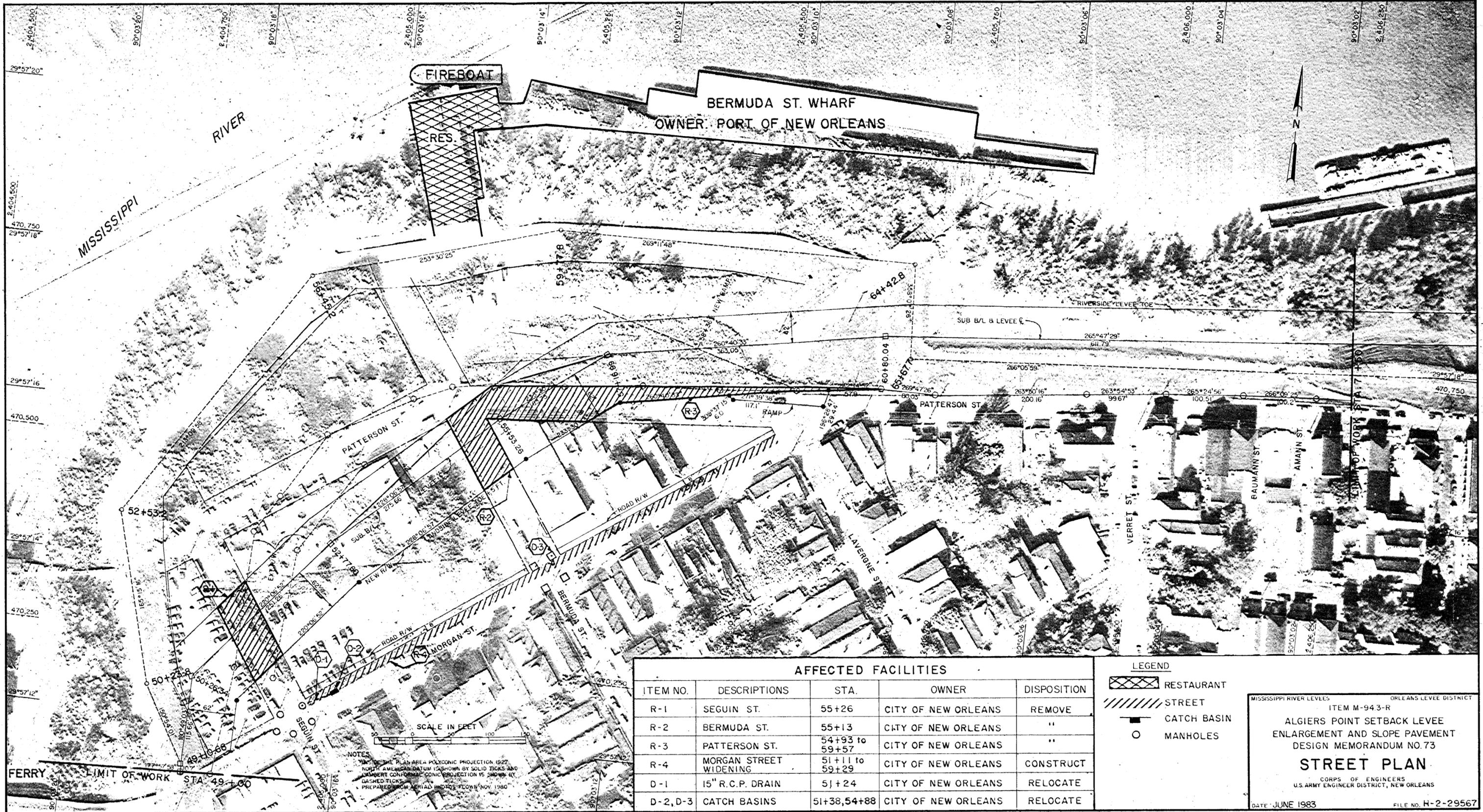
RIVERSIDE

LANDSIDE



NOTE:
 All stations in tabulations on plates 2 thru 7 are referenced to sub baseline of new levee

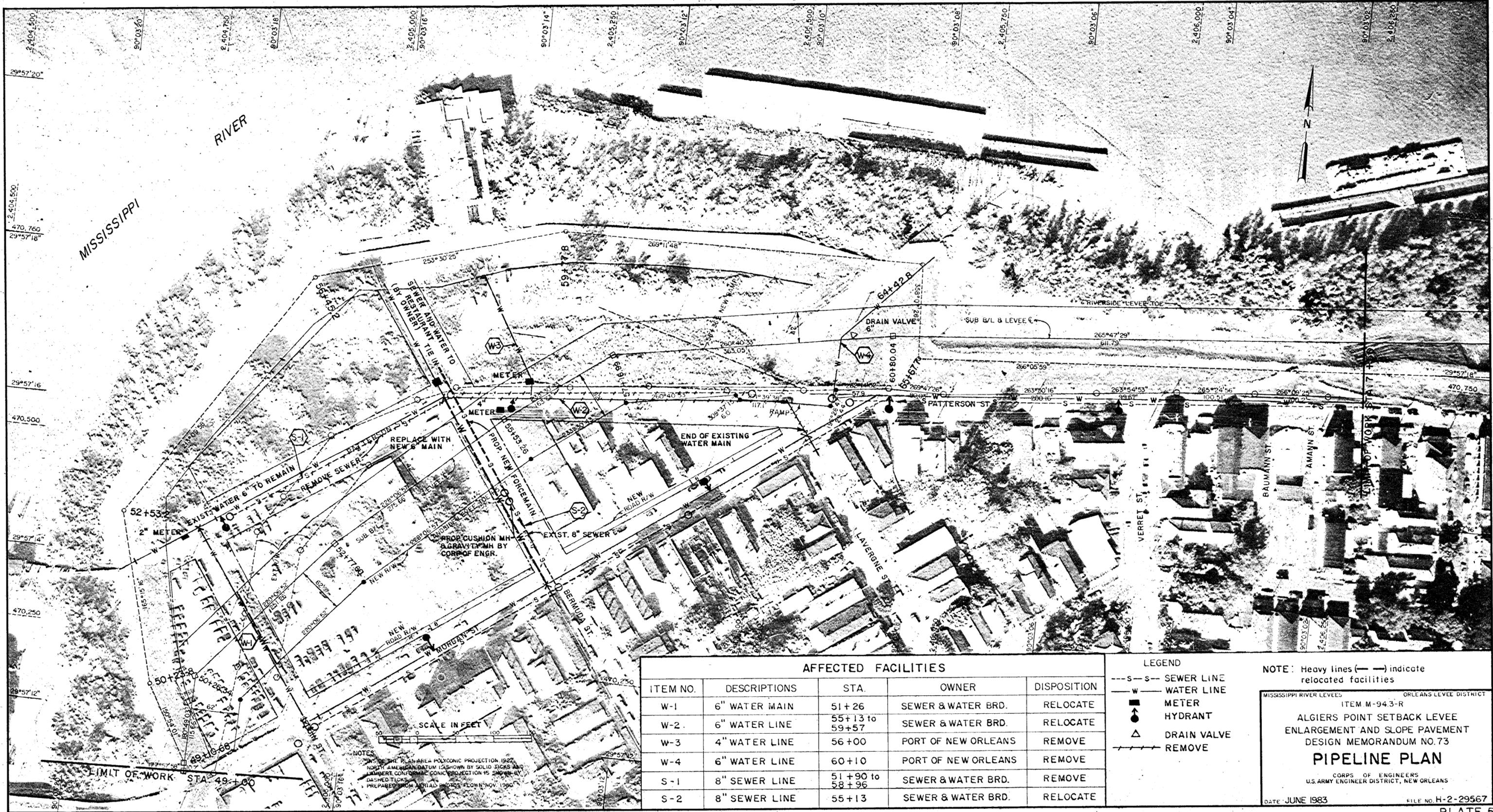
MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
 ITEM M-94.3-R
 ALGIERS POINT SETBACK, LEVEE ENLARGEMENT AND SLOPE PAVEMENT
 DESIGN MEMORANDUM NO.73
LEVEE SECTIONS
 CORPS OF ENGINEERS
 U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
 DATE: JUNE 1983 FILE NO. H-2-29567



AFFECTED FACILITIES				
ITEM NO.	DESCRIPTIONS	STA.	OWNER	DISPOSITION
R-1	SEGUIN ST.	55+26	CITY OF NEW ORLEANS	REMOVE
R-2	BERMUDA ST.	55+13	CITY OF NEW ORLEANS	"
R-3	PATTERSON ST.	54+93 to 59+57	CITY OF NEW ORLEANS	"
R-4	MORGAN STREET WIDENING	51+11 to 59+29	CITY OF NEW ORLEANS	CONSTRUCT
D-1	15" R.C.P. DRAIN	5J+24	CITY OF NEW ORLEANS	RELOCATE
D-2, D-3	CATCH BASINS	51+38, 54+88	CITY OF NEW ORLEANS	RELOCATE

LEGEND	
	RESTAURANT
	STREET
	CATCH BASIN
	MANHOLES

MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
 ITEM M-94 3-R
 ALGIERS POINT SETBACK LEVEE
 ENLARGEMENT AND SLOPE PAVEMENT
 DESIGN MEMORANDUM NO. 73
STREET PLAN
 CORPS OF ENGINEERS
 U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
 DATE: JUNE 1983 FILE NO. H-2-29567



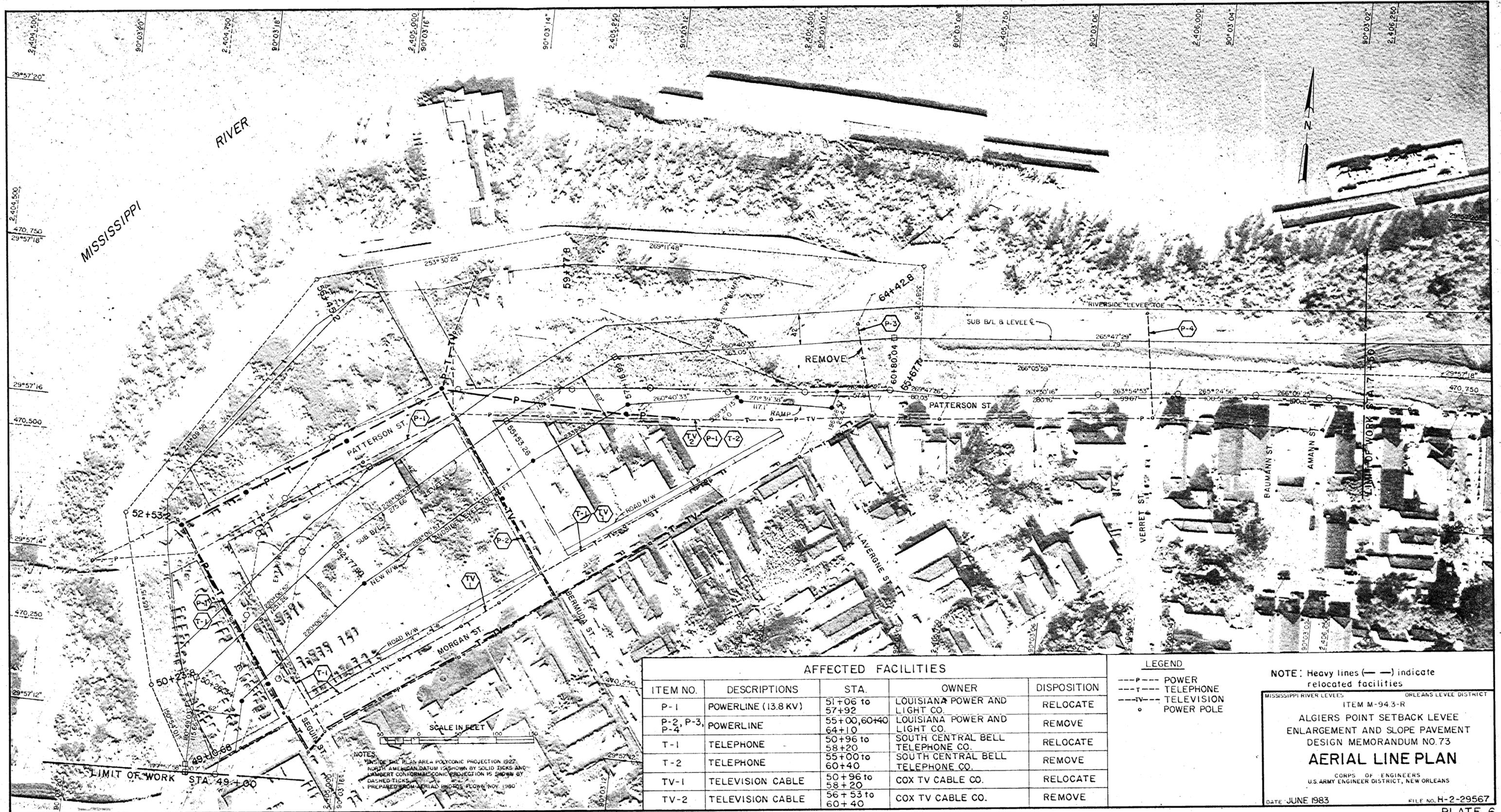
AFFECTED FACILITIES				
ITEM NO.	DESCRIPTIONS	STA.	OWNER	DISPOSITION
W-1	6" WATER MAIN	51+26	SEWER & WATER BRD.	RELOCATE
W-2	6" WATER LINE	55+13 to 59+57	SEWER & WATER BRD.	RELOCATE
W-3	4" WATER LINE	56+00	PORT OF NEW ORLEANS	REMOVE
W-4	6" WATER LINE	60+10	PORT OF NEW ORLEANS	REMOVE
S-1	8" SEWER LINE	51+90 to 58+96	SEWER & WATER BRD.	REMOVE
S-2	8" SEWER LINE	55+13	SEWER & WATER BRD.	RELOCATE

- LEGEND
- s--- SEWER LINE
 - w--- WATER LINE
 - METER
 - HYDRANT
 - ▲ DRAIN VALVE
 - /--- REMOVE

NOTE: Heavy lines (—) indicate relocated facilities

MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
 ITEM M-943-R
 ALGIERS POINT SETBACK LEVEE
 ENLARGEMENT AND SLOPE PAVEMENT
 DESIGN MEMORANDUM NO. 73
PIPELINE PLAN
 CORPS OF ENGINEERS
 U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS

DATE: JUNE 1983 FILE NO. H-2-29567



AFFECTED FACILITIES				
ITEM NO.	DESCRIPTIONS	STA.	OWNER	DISPOSITION
P-1	POWERLINE (13.8 KV)	51+06 to 57+92	LOUISIANA POWER AND LIGHT CO.	RELOCATE
P-2, P-3, P-4	POWERLINE	55+00, 60+40, 64+10	LOUISIANA POWER AND LIGHT CO.	REMOVE
T-1	TELEPHONE	50+96 to 58+20	SOUTH CENTRAL BELL TELEPHONE CO.	RELOCATE
T-2	TELEPHONE	55+00 to 60+40	SOUTH CENTRAL BELL TELEPHONE CO.	REMOVE
TV-1	TELEVISION CABLE	50+96 to 58+20	COX TV CABLE CO.	RELOCATE
TV-2	TELEVISION CABLE	56+53 to 60+40	COX TV CABLE CO.	REMOVE

LEGEND	
---P---	POWER
---T---	TELEPHONE
---TV---	TELEVISION
○	POWER POLE

NOTE: Heavy lines (—) indicate relocated facilities

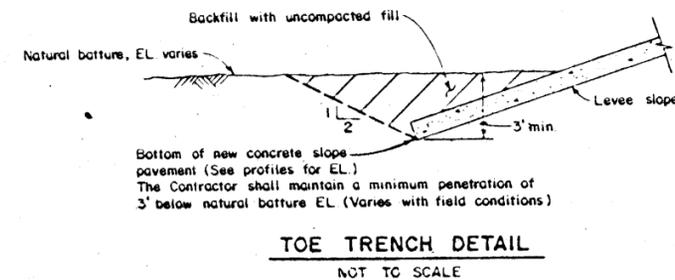
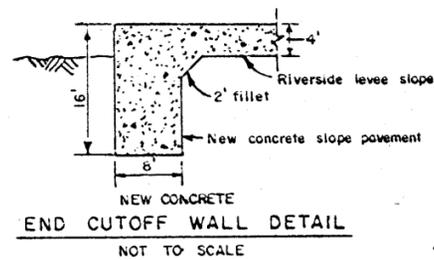
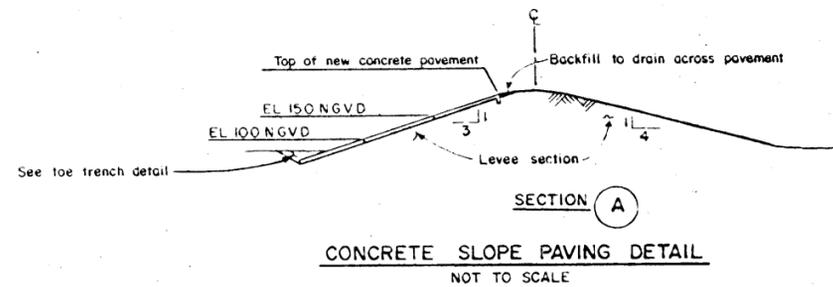
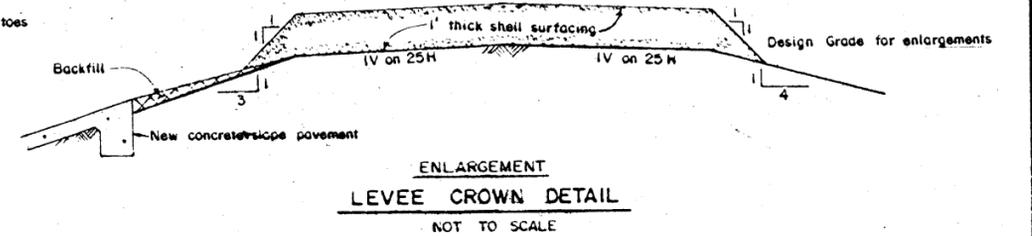
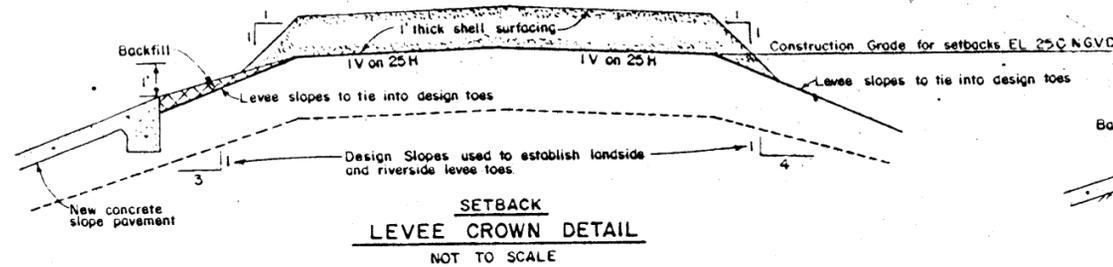
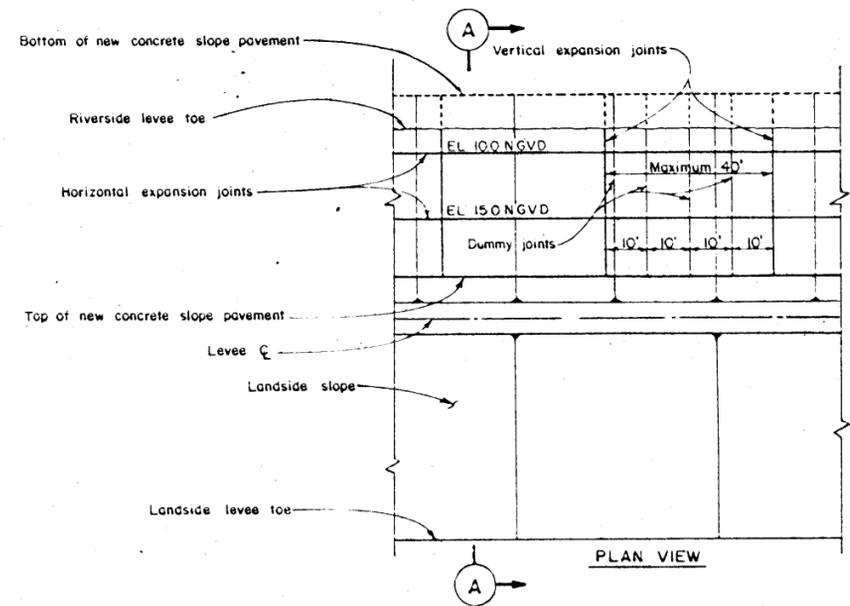
MISSISSIPPI RIVER LEVEES ORLEANS LEVEL DISTRICT
ITEM M-943-R

ALGIERS POINT SETBACK LEVEE ENLARGEMENT AND SLOPE PAVEMENT DESIGN MEMORANDUM NO.73

AERIAL LINE PLAN

CORPS OF ENGINEERS
U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS

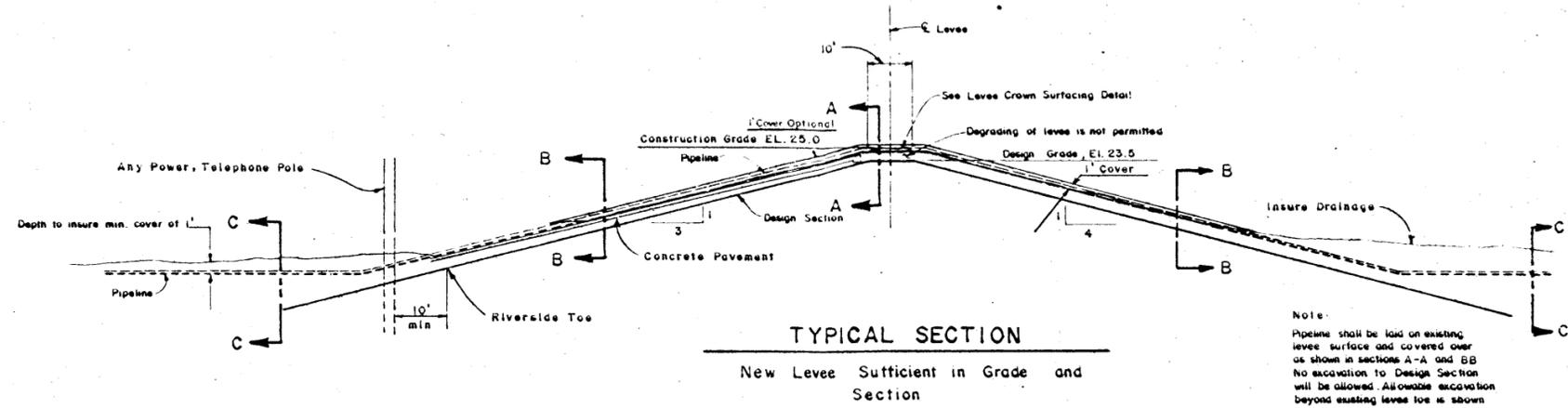
DATE: JUNE 1983 FILE NO. H-2-29567



MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
ITEM M-94.3-R
ALGIERS POINT SETBACK, LEVEE
ENLARGEMENT AND SLOPE PAVEMENT
DESIGN MEMORANDUM NO. 73
MISC. DETAILS
CORPS OF ENGINEERS
U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
DATE: JUNE 1983 FILE NO. H-2-29567

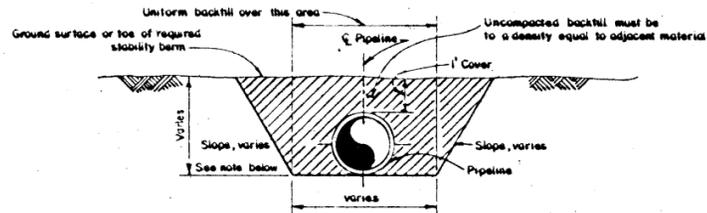
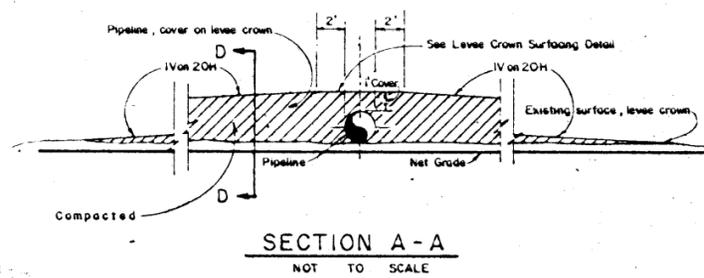
RIVERSIDE

LANDSIDE



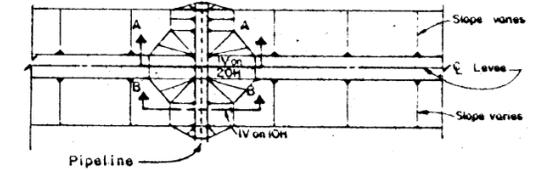
NOTES:

1. Smooth transitions shall be constructed between the levee enlargement and existing levee.
2. All fresh fills shall be sodded or fertilized and seeded and shall be maintained until a healthy growth is obtained.
3. The crown ramp over the pipeline crossing shall be surfaced with roadway washed gravel six (6) inches in thickness or shell nine (9) inches in thickness for full width (10' min.) and length of ramp.
4. Fill material used in construction of levee enlargement ramps pipe cover, and backfill of excavation shall be impervious earth fill.
5. Pipeline markers shall be placed and maintained at each toe of levee in line with pipe crossing and indicating owner, size, and number of lines, product and address for contracting owner.

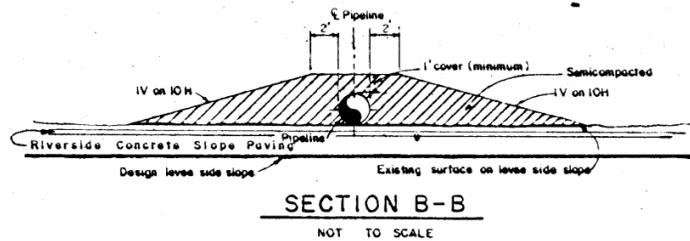


NOTE: Maximum depth allowed is that required to provide 1 foot of cover over pipeline or allowed by stability analysis.

SECTION C-C
NOT TO SCALE

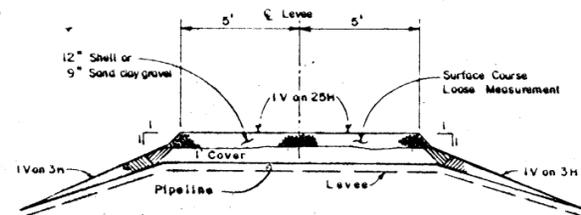


TYPICAL PLAN
NOT TO SCALE



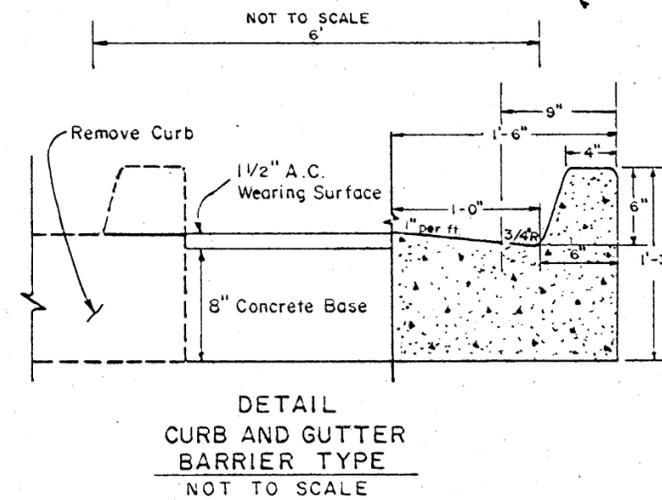
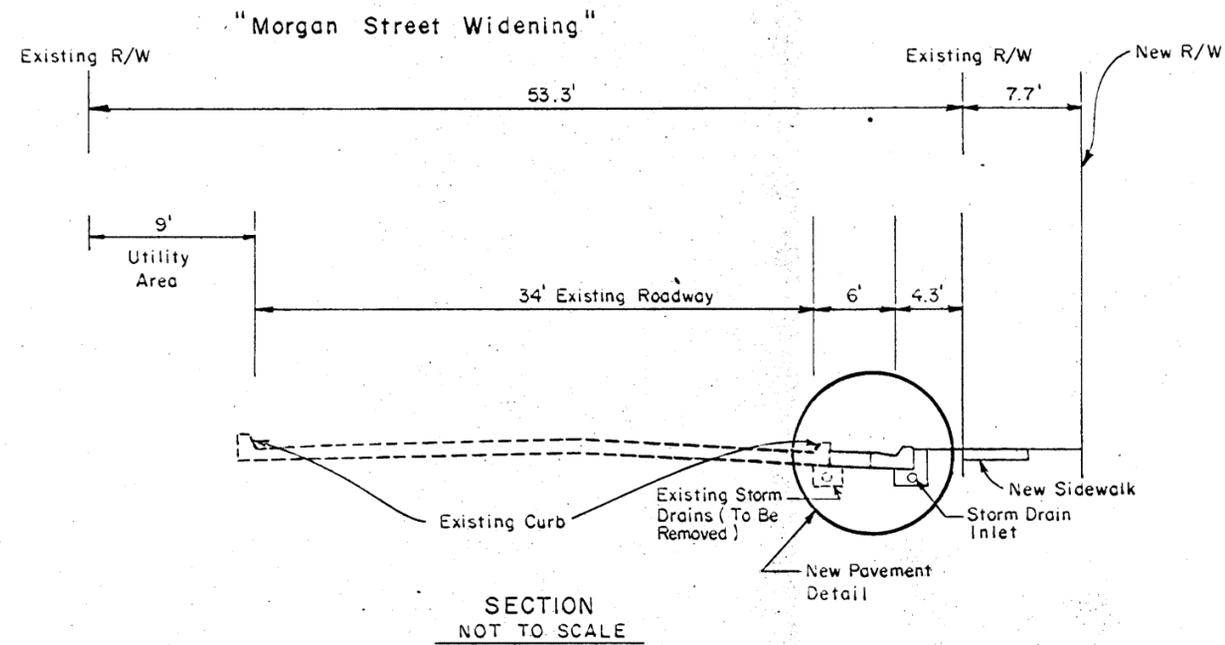
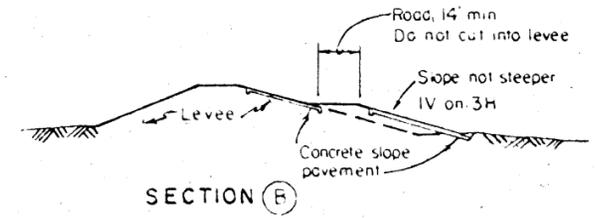
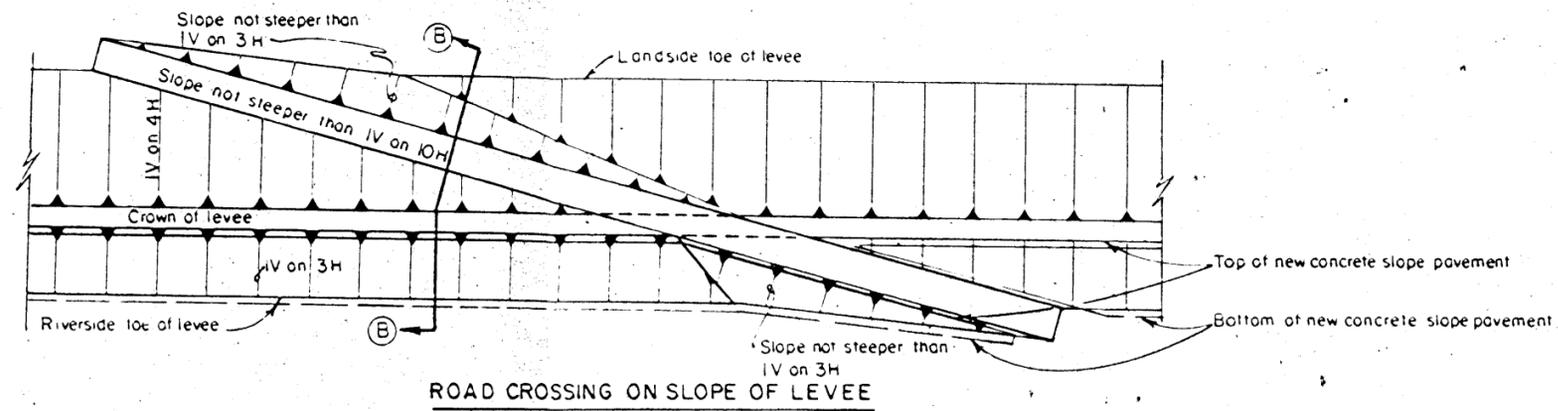
SECTION B-B
NOT TO SCALE

NOTE: Riverside cover is at owners option



SECTION D-D
NOT TO SCALE

MISSISSIPPI RIVER LEVES ORLEANS LEVEE DISTRICT
ITEM M-94.3-R
*ALGIERS POINT SETBACK, LEVEE ENLARGEMENT AND SLOPE PAVEMENT
DESIGN MEMORANDUM NO.73
CROSSING DETAIL
CORPS OF ENGINEERS
U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
DATE: JUNE 1983 FILE NO. H-2-29567



MISSISSIPPI RIVER LEVEES ORLEANS LEVEE DISTRICT
 ITEM M-94.3-R
 ALGIERS POINT SETBACK, LEVEE
 ENLARGEMENT AND SLOPE PAVEMENT
 DESIGN MEMORANDUM NO.73
STREET, RAMP AND CURB DETAIL
 CORPS OF ENGINEERS
 U.S. ARMY ENGINEER DISTRICT, NEW ORLEANS
 DATE: JUNE 1983 FILE NO. H-2-29567

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES
WESTBANK ORLEANS PARISH
LOUISIANA

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT

RELOCATION DESIGN MEMORANDUM NO. 73

APPENDIX I

ATTORNEY'S REPORT

ATTORNEY'S INVESTIGATION
AND
REPORT OF COMPENSABLE INTEREST

Item M-94.3-R
Algiers Point Levee Setback
and Enlargement

This investigation and report is made in accordance with DIVR 1110-2-2 (see also DIVR 1110-2-1, 29 January 1968) and ER 1180-1-1, Section 73, Part 3, 73-203 and 73-204 on the following facilities which will be affected by the subject project:

a. Three powerlines, one crossing proposed levee at station 60+40, another crossing existing levee at station 64+00, and a 13.8kv distribution powerline parallel to and landside of Patterson Street looping parallel to and riverside of Seguin Street owned by Louisiana Power and Light Company with service drops to Bermuda Street Wharf and Brake Tag Station.

b. A telephone line, with service drops parallel to and riverside of Morgan Street looping parallel to Seguin Street and crossing the existing levee at Patterson Street owned by South Central Bell Telephone Company.

c. Two cable television lines, one parallel to Morgan Street from approximate station 50+96 to station 58+20, and a cable television line, parallel to Patterson Street on Louisiana Power and Light poles from approximate station 60+40 to station 56+53 both owned by Cox Cable.

d. The following facilities owned by the Sewerage and Water Board:

i. A 6" main parallel to Patterson station from 52+00, hydrant location, to manhole station 60+00.

ii. A 6" main parallel to Seguin Street.

iii. A sewer main parallel to Patterson Street from station 52+00 to station 60+00.

iv. A sewer main parallel to Bermuda Street crossing the new levee at approximate station 55+00.

e. The following facilities owned by Streets Department, City of New Orleans:

i. A paved roadway perpendicular to new levee crossing at approximate station 51+00 Seguin Street, station 55+00 Bermuda Street and oblique to new levee from station 55+00 to station 60+00, Patterson Street.

ii. Morgan Street from Seguin Street to Lavergne Street.

iii. Storm drains located at the corners of Morgan and Bermuda Streets (station 54+88) and under Morgan Street from station 51+10 to station 51+38.

f. The following facilities owned by New Orleans Public Service, Inc.:

i. A 3" gas line parallel to Bermuda Street crossing the new levee at approximate station 55+00.

ii. A 1" line parallel to Patterson Street oblique to new levee from station 54+00 to station 57+00.

g. The following facilities owned by the Board of Commissioners Port of New Orleans:

i. A chain link fence located from approximate station 55+00 to station 61+00.

ii. A paved ramp at approximate station 59+70.

iii. A 4" waterline parallel to the paved ramp at station 60+10.

iv. A 6" water line parallel to the paved ramp at station 60+10.

h. The following facilities owned by the City of New Orleans:

i. A brake tag station with additional facilities for a Council on Aging senior citizen meeting room and office (station 55+55).

ii. Concrete retaining wall and steps, and two shell ramps located from approximate levee station 64+00 through limit of work station 66+92.

The location of the above facilities are indicated on Plates 3 through 7, entitled, "Item M-94.3-R, Algiers Point Setback, Levee Enlargement and Slope Pavement, RDM No. 73." *e/ 5-1-71*

I.
COMPENSABLE INTEREST
OF
LOUISIANA POWER AND LIGHT COMPANY (LP&L)

LP&L is a Louisiana public utility domiciled in Orleans Parish. It provides electric service throughout southeast Louisiana and has the right of

eminent domain. See LA. REV. STAT. ANN. 19:2(7). The facility listed in "a" above is owned and maintained by LP&L. It is located both inside and outside the existing levee right-of-way.†

Since the portion of the facility located inside the existing right-of-way is there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, LP&L has no compensable interest where the facility lies within the existing right-of-way.

There has been no evidence furnished to date that the portion of the right-of-way outside the existing levee right-of-way was purchased from the fee owners. However, a public utility through the power of eminent domain possesses a real right in the right-of-way in need of relocation, even without the use of conventional negotiation and acquisition of the subject right-of-way through the Doctrine of Unopposed Use. See LA. REV. STAT. ANN. 19:2(7) and 19:14.

Therefore, LP&L does have a compensable interest where the facility is located outside the existing levee right-of-way.

II.
COMPENSABLE INTEREST
OF
SOUTH CENTRAL BELL TELEPHONE COMPANY

South Central Bell Telephone Company is a Delaware corporation licensed to do business in the State of Louisiana. The company is considered to be a public utility by virtue of LA. REV. STAT. ANN. 45:781. Pursuant to this authority, South Central Bell possesses the right of eminent domain, LA. REV. STAT. ANN. 19:2(6). The facility listed in "b" is owned and maintained by South Central Bell Telephone Company. It is located both inside and outside the existing levee right-of-way.

Since the portion of the facility located inside the existing right-of-way is there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, South Central Bell has no compensable interest where the facility lies within the existing right-of-way.

There has been no evidence furnished to date that the portion of the right-of-way outside the existing levee right-of-way was purchased from the

†The term existing levee right-of-way refers to the levee right-of-way acquired prior to 1976.

fee owners. However, a public utility through the power of eminent domain possesses a real right in the right-of-way in need of relocation, even without the use of conventional negotiation and acquisition of the subject right-of-way through the Doctrine of Unopposed Use. See LA. REV. STAT. ANN. 19:2(6) and 19:14.

Therefore, South Central Bell does have a compensable interest where the facility is located outside the existing levee right-of-way.

III.
COMPENSABLE INTEREST
OF
COX CABLE NEW ORLEANS, INCORPORATED

Cox Cable New Orleans, Incorporated is a Louisiana corporation domiciled in Orleans Parish, Louisiana. The subject cable service is not considered to be a public utility, although it does have the right of eminent domain. KNOK-CATV, Inc. v. Louisiana Cable TV, Inc., 195 So. 2d 297.

The facility owned by Cox Cable is located on power poles owned by LP&L by virtue of a permit. The power poles are located both inside and outside of the existing levee right-of-way. The portion of the facility located inside the existing right-of-way is there through permission granted by the local levee authority. Since Cox Cable located the facility there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, Cox Cable has no compensable interest where the facility lies within the existing right-of-way.

As to the portion of the facility located outside the existing levee right-of-way, Cox Cable, if expelled by LP&L, could expropriate land to establish and maintain its own poles or otherwise. Thus, Cox Cable has a compensable interest in its facility located on power poles that belong to LP&L.

Since Cox Cable is not the owner of property, its facilities outside of the existing levee right-of-way will be acquired under Public Law 91-646.

IV.
COMPENSABLE INTEREST
OF
THE SEWERAGE AND WATER BOARD
OF THE CITY OF NEW ORLEANS

The Sewerage and Water Board of the City of New Orleans is a subdivision of the State of Louisiana. It was authorized pursuant to the authority of LA. REV. STAT. ANN. 33:4093. It has the power of eminent domain through the authority granted by LA. REV. STAT. ANN. 19:1.

The facilities owned by the Sewerage and Water Board are located both inside and outside the existing levee right-of-way. The facilities and portions of facilities located inside the existing right-of-way is there through permission granted by the local levee authority. Since the Sewerage and Water Board located the facilities there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, the Sewerage and Water Board has no compensable interest where the facilities lie within the existing right-of-way.

However, where the facilities within the existing right-of-way are serving a valid governmental function, applicable law authorizes their relocation when they are serving in this capacity.

There has been no evidence furnished to date that the right-of-way for the facilities listed in "d" belonging to the Sewerage and Water Board was purchased from the underlying fee owners. However, a political subdivision having the power of eminent domain acquires a real right in the real estate of the facilities to be located through the Doctrine of Unopposed Use. Under this Doctrine, when a political corporation constructs facilities upon, under, or over property with the consent or acquiescence of the owner of the property such owner shall be deemed to have waived his right to contest or object to the taking of such property for public purposes, LA. REV. STAT. ANN. 19:14 and 19:1.

Accordingly, the Sewerage and Water Board does have a compensable interest where the facilities are located outside the existing levee right-of-way.

V.
COMPENSABLE INTEREST
OF
THE CITY OF NEW ORLEANS
THROUGH
THE STREETS DEPARTMENT

The City of New Orleans is a municipality of the State of Louisiana. As a municipality, it holds in trust for the public fee title to the streets of the city. As a municipality, it is also vested with the right of eminent domain, LA. REV. STAT. ANN. 33:4621.

The facilities owned by the City of New Orleans through its Streets Department are located both inside and outside the existing levee right-of-way. The facilities and portions of facilities located inside the existing right-of-way are there through permission granted by the local levee authority. Since the City of New Orleans located the facilities there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, the City of New Orleans has no compensable interest where the facilities lie within the existing levee right-of-way.

However, where the facilities within the existing right-of-way are serving a valid governmental function, applicable law authorizes their relocation when they are serving in this capacity.

The facilities owned by the City of New Orleans through the Streets Department are all located outside the existing levee right-of-way. There has been no evidence furnished to date that the City of New Orleans owns fee title to the streets. However, a municipality having the right of eminent domain acquires a real right to the property due to consent or acquiescence of the landowner by virtue of the provisions of LA. REV. STAT. ANN. 33:4621 and 19:41. Thus, the City of New Orleans has a real right to the streets affected by the project.

Accordingly, the City of New Orleans through the Streets Department has a compensable interest in the streets affected by the subject project.

VI.
COMPENSABLE INTEREST
OF
NEW ORLEANS PUBLIC SERVICE, INCORPORATED

New Orleans Public Service is a Louisiana public utility domiciled in Orleans Parish. It provides electric and gas service throughout the Parish of Orleans. As a public utility, it has the right of eminent domain, LA. REV. STAT. ANN. 19:2(7).

The facilities listed in "f" are owned and maintained by New Orleans Public Service, Incorporated. They are located both inside and outside the existing levee right-of-way.

Since New Orleans Public Service, Incorporated located portions of the facilities inside the existing right-of-way through permission from the local levee authority, and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, New Orleans Public Service, Incorporated has no compensable interest where the facilities lie within the existing right-of-way.

There has been no evidence furnished to date that the portion of the right-of-way outside the existing levee right-of-way was purchased from the fee owners. However, a public utility through the power of eminent domain possesses a real right in the right-of-way in need of relocation, even without the use of conventional negotiation and acquisition of the subject right-of-way through the Doctrine of Unopposed Use. See LA. REV. STAT. ANN. 19:2(7) and 19:14.

Therefore, New Orleans Public Service, Incorporated does have a compensable interest where the facility is located outside the existing levee right-of-way.

VII.
COMPENSABLE INTEREST
OF
THE BOARD OF COMMISSIONERS
PORT OF NEW ORLEANS

The Board of Commissioners for the Port of New Orleans is authorized pursuant to authority granted in LA. REV. STAT. ANN. 34:1. It is a political

subdivision of the State of Louisiana and is charged with regulating commerce and traffic in the port and harbor of New Orleans, LA. REV. STAT. ANN. 34:21. Pursuant to this authority it is charged with providing light, water, police protection, and other services for wharves, landing sheds as it deems advisable, LA. REV. STAT. ANN. 34:21. It also has the power of eminent domain, LA. REV. STAT. ANN. 34:23.

The facilities owned by the Port Commission are located both inside and outside the existing levee right-of-way. The facilities and portions of facilities located inside the existing right-of-way are there through permission granted by the local levee authority. Since the Port Commission located the facilities there through permission and is being asked to relocate for levee construction, it has not acquired a real right to the property. Therefore, the Port Commission has no compensable interest where the facilities lie within the existing right-of-way.

However, where the facilities within the existing right-of-way are serving a valid governmental function, applicable law authorizes their relocation when they are serving in this capacity.

There has been no evidence furnished to date that the Port Commission owns fee title to the facilities existing outside of the levee right-of-way. However, a political subdivision having the right of eminent domain acquires a real right to property due to consent or acquiescence of the landowner by virtue of the provisions of LA. REV. STAT. ANN. 33:4621 and 19:14. Thus, the Port Commission has a compensable interest in the facilities where they are located outside the existing levee right-of-way.

VIII.
COMPENSABLE INTEREST
OF
THE CITY OF NEW ORLEANS

The City of New Orleans is a municipal corporation of the State of Louisiana and, as such, has the power of eminent domain. See LA. REV. STAT. ANN. 19:102.

The facilities owned by the City of New Orleans are located both inside and outside the existing levee right-of-way. The facilities and portions of facilities located inside the existing right-of-way are there through permission granted by the local levee authority. Since the City of New Orleans located the facilities there through permission and is being asked to

relocate for levee construction, it has not acquired a real right to the property. Therefore, the City has no compensable interest where the facilities lie within the existing right-of-way.

However, where the facilities within the existing right-of-way are serving a valid governmental function, applicable law authorizes their relocation when they are serving in this capacity.

There has been no evidence furnished to date that the City of New Orleans owns fee title to the facilities existing outside of the levee right-of-way. However, a municipality having the right of eminent domain acquires a real right to property in need of relocation, even without the use of conventional negotiation and acquisition of the subject right-of-way. See LA. REV. STAT. ANN. 19:115. Thus, the City of New Orleans has a compensable interest in the facilities where they are located outside the existing levee right-of-way.

IX. AUTHORITY AND OBLIGATION

Authority for construction of levee setbacks of main line Mississippi River Levees is contained in the Act of 15 May 1928 (PL 391, 70th Congress), as amended. Payment or reimbursement of costs to local cooperating agencies for rights-of-way furnished, including necessary relocation or alteration of public roads, highways, railroads, public utilities, and pipelines is authorized by the Flood Control Act of 18 August 1941 (PL 228, 77th Congress, 1st Session), as amended by the Flood Control Act of 22 December 1944 (PL 534, 78th Congress, 2d Session).

In those cases where the local cooperating agency is not financially able to arrange for relocation or alteration of interfering facilities, subject to reimbursement of costs, the District Engineer is authorized to enter into a contract with the facility owner, subject to the requirements of ER 1180-1-1.

Under the provisions of Section 111 of 72 Stat. 303, as amended by Section 309 of 79 Stat. 1094 (33 U.S.C. 633), the Chief of Engineers may, in civil works projects, protect, alter, reconstruct, relocate or replace any structure or facility owned by an agency of Government (State, county, city, or town or any legally created subdivision thereof) and utilized in the performance of a governmental function.

Pursuant to authority granted by PL 91-646, EP 405-1-2, Section 3, Paragraph 6-14 (a)(6) allows for the removal, reinstallation, reestablishment,

including such modification as deemed necessary by the District Engineer, and reconnection of utilities for machinery, equipment, appliances, or other items, not acquired as real property.

X.
SUMMARY

It is the obligation of the local cooperating agency subject to reimbursement by the United States to relocate all of the above facilities located outside the existing levee right-of-way or those portions thereof which are located within existing levee right-of-way and are being utilized in the performance of governmental functions. It is the obligation of the local cooperating agency subject to reimbursement by the Federal Government to relocate the facility listed in "c" as a Uniform Relocation Assistance item. All other facilities or portions thereof will be relocated at non-Federal expense. Any facility which is considered to be abandoned will be removed with just compensation being the cost of removal.

31 March 1983

DATE

R.D. Florent

RANDY D. FLORENT
Attorney-Advisor
U. S. Army Engineer District
New Orleans

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES
WESTBANK ORLEANS PARISH
LOUISIANA

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT

RELOCATION DESIGN MEMORANDUM NO. 73

APPENDIX II
COST ESTIMATES

TABLE 3 - DETAILED COST ESTIMATE
CITY OF NEW ORLEANS - STREETS

(OCTOBER 1982 PRICE LEVELS)

Description	Unit	Quantity	Unit Cost	Amount
<u>Item R-1, Seguin St.</u>				
Removal of Pavement	LF	120	40	\$4,800
<u>Item R-2, Bermuda St.</u>				
Removal of Pavement	LF	120	40	\$4,800
<u>Item R-3, Patterson St.</u>				
Removal of Pavement	LF	500	40	\$20,000
<u>Item R-4, Morgan St.</u>				
<u>Widening</u>				
Cleaning & Grubbing	LS	1	7,000	7,000
Grading	LS	1	1,000	1,000
8" Thick concrete Base	CY	104	125	13,000
1 1/2" Asphaltic Concrete wearing course	Tons	39	40	1,560
4" Thick Concrete Walk	LF	650	7	4,550
Removal of Existing Curb and Gutter	LF	680	5	3,400
New Curb and Gutter	LF	680	8	5,440
			Subtotal Item R-4	\$35,950
			Subtotal of Item R-1, R-2, R-3, & R-4	\$65,550
			Contingencies (20%+)	16,450
			Subtotal	\$82,000
			E&D (6%+)	5,000
			S&A (6%+)	5,000
			TOTAL	\$92,000

Note: Morgan Street R/W cost is \$46,000

(5,200 S.F. x \$7.00 = 36,400 + 9,600 for contingencies = \$46,000)

Ref. LMNRE-E Identification No. 80830 for unit value (\$7.00 per S.F.).

TABLE 4 - DETAILED COST ESTIMATE
CITY OF NEW ORLEANS - DRAINAGE FACILITIES

(OCTOBER 1982 PRICE LEVELS)

Description	Unit	Quantity	Unit Cost	Amount
Item D-1 (Relocate) 15" R.C.P.	LF	56	25	\$1,400
Item D-2 (Relocate) Catch Basin	Each	1	1,000	1,000
Item D-3 (Relocate) Catch Basin	Each	2	800	<u>1,600</u>
			Subtotal	\$4,000
			Contingencies (25%+)	<u>1,000</u>
			Subtotal	\$5,000
			E&D (6%+)	300
			S&A (6%+)	<u>300</u>
			TOTAL	<u>\$5,600</u>

TABLE 5 - DETAILED COST ESTIMATE
SS&WB FACILITIES - WATERLINES AND SEWERLINES

(OCTOBER 1982 PRICE LEVELS)

Description	Unit	Quantity	Unit Cost	Amount
Item W-1				
Remove 6" Waterline	L.F.	120	5.00	600.00
Construct 6" Waterline	L.F.	160	45.00	7,200.00
Item W-2				
Remove 6" Waterline	L.F.	500	5.00	2,500.00
Construct 6" Waterline	L.F.	340	50.00	17,000.00
Item S-1				
Remove 8" Sewerline	L.F.	750	10.0	7,500.00
Item S-2				
Remove 8" Sewerline	L.F.	130	10.0	1,300.00
Construct 8" force main	L.F.	160	60.0	9,600.00
Construct Sewer M.H.	Each	2	1,500.00	3,000.00
Subtotal				\$48,700.00
Contingencies 25%+				12,300.00
Subtotal				\$61,000.00
E&D (6%+)				3,650.00
S&A (6+)				3,650.00
				<u>\$68,300.00</u>

TABLE 6 - DETAILED COST ESTIMATE
N.O. PROPERTY MANAGEMENT FACILITIES

(OCTOBER 1982 PRICE LEVELS)

(1) Brake Tag Station (Item B-1)

(a) Lands & Damages

	<u>SF</u>	<u>Unit Value</u>	<u>Total Value</u>
<u>Fee</u>			
Residential/Commercial	12,000	\$2.50	\$ 30,000
Improvements			120,000
Severance Damage			<u>0</u>
Total (R)			\$150,000
Contingencies 25% (R)			38,000

(b) Acquisition Costs (Estimated 1 tract)

Non-Federal 1 @ \$1,400 per tract (R)			1,000
Federal 1 @ 700 per tract (R)			1,000

(c) PL-91-646 10,000

(d) Total Estimated Real Estate Cost \$200,000

Note: The Algiers Brake Tag Station is located at the intersection of Morgan and Bermuda Streets in New Orleans, Louisiana. This estimate is for the relocation cost of the Brake Tag Station under Section 111 of P.L. 85-500.

The improvement value in this estimate is for a new, pre-engineered steel building with a concrete floor.

(2) 3rd Dist Ferry Landing (Item L-1)

<u>Description</u>	<u>Quantity</u>	<u>Unit Cost</u>	<u>Amount</u>
Removal of item L-1	1	Lump	\$ 8,000
		Contingencies (25%+)	2,000
		<u>TOTAL</u>	<u>\$10,000</u>

TABLE 7 - DETAILED COST ESTIMATE
(OCTOBER 1982 PRICE LEVELS)

PORT OF NEW ORLEANS FACILITIES

Description	Unit	Quantity	Unit Cost	Amount
Item W-3				
Remove 4" Waterline	L.F.	30	5.00	150.00
Item W-4				
Remove 6" Waterline	L.F.	130	5.00	650.00
Item F-1				
Remove chain link fence	L.F.	800	1.00	<u>800.00</u>
			Subtotal	\$1,600.00
Item R-5				
Removal of paved ramp	L.F.	160	40.00	<u>\$6,400.00</u>
			Subtotal	\$8,000.00
			Contingencies (25+)	<u>2,000.00</u>
			Subtotal	\$10,000.00
			E&D (6%+)	600.00
			S&A (6%+)	<u>600.00</u>
				\$11,200.00

TABLE 8 - DETAILED COST ESTIMATE
LOUISIANA POWER AND LIGHT COMPANY - POWERLINES

(OCTOBER 1982 PRICE LEVELS)

Description	Amount
Items P-1, P-2, and P-3; relocate, remove or adjust poles, guys, anchors, conductors, transformers, etc. to accommodate the project.	\$12,600
Contingencies (25%+)	3,150
Subtotal	\$15,750
E&D (6%+)	1,000
S&A (6%+)	1,000
TOTAL	\$17,750

Note: Cost of removing item P-4, which is in enlargement limits, is not significant.

TABLE 9 - DETAILED COST ESTIMATE
SOUTH CENTRAL BELL TELEPHONE COMPANY - TELEPHONE

(OCTOBER 1982 PRICE LEVELS)

<u>Description</u>	<u>Amount</u>
Removal of items T-1 and T-2	\$ 1,130
Reconstruction of items T-1, T-2	\$ 6,870
Cost of sharing poles with LP&L	<u>\$ 7,000</u>
	Subtotal \$15,000*
	E&D (6%+) 9,00
	S&A (6%+) 9,00
	<u>Total \$16,800</u>

*This includes 25% Contingencies.

TABLE 10 - DETAILED COST ESTIMATE
 NOPSI - GASLINES

(OCTOBER 1982 PRICE LEVELS)

Description	Unit	Quantity	Unit Cost	Amount
Item G-1 Relocate				
Remove 3" gas line	LF	160	5.00	\$ 800
Construct 3" gas line	LF	200	30	6,000
Item G-2				
Remove 1" gas line	LF	330	5.0	<u>1,650</u>
			Subtotal	\$ 8,450
			Contingencies (25%+)	<u>2,112</u>
			Subtotal	\$10,562
			E&D (6%+)	619
			S&A (6%+)	<u>619</u>
			TOTAL	\$11,800

FLOOD CONTROL
MISSISSIPPI RIVER AND TRIBUTARIES
MISSISSIPPI RIVER LEVEES
WESTBANK ORLEANS PARISH
LOUISIANA

ITEM M-94.3-R
ALGIERS POINT SETBACK
AND LEVEE ENLARGEMENT

RELOCATION DESIGN MEMORANDUM NO. 73

APPENDIX III

SPECIAL STATUTORY AUTHORITY DATA



ERNEST N. MORIAL
MAYOR

CITY OF NEW ORLEANS
DEPARTMENT OF SAFETY AND PERMITS
DIRECTOR'S OFFICE
ROOM 7E05 CITY HALL
NEW ORLEANS, LA. 70112

EVELYN PUGH
DIRECTOR

June 6, 1983

LMNED - SR

Attention: Mr. Ray Zimny
P. O. Box 60267
New Orleans, La. 70160

RE: Algiers Brake Tag Station

Dear Mr. Zimny:

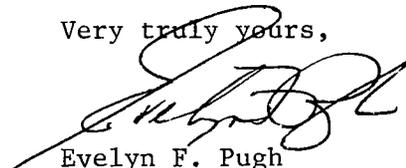
Pursuant to our recent conversation concerning the functions and operation of the Algiers Brake Tag Station, this letter will confirm the City of New Orleans' continuing need for the operation of a motor vehicle inspection station in Algiers. The Motor Vehicle Inspection Bureau is an established division within the Department of Safety and Permits and, in accordance with the Code of the City of New Orleans (Ordinance No. 828 M.C.S., as amended), it inspects and tests all vehicles brought to any inspection station maintained by the Bureau and determines whether such vehicles are in safe operating condition and are equipped with all safety devices required by law.

The Algiers station is only one of the three inspection stations operated by the City of New Orleans, all of which are open for business and perform inspection services throughout the year. An average of approximately 23,000 - 25,000 inspections are performed at the Algiers station annually. For the months January through April 1983, 7918 vehicles were inspected at the Algiers station, as compared to 7580 for the same period in 1982.

The Algiers Brake Tag Station performs a valuable and necessary service to the City of New Orleans on a continuing basis and must be replaced as quickly as possible.

Thank you for your continued cooperation. Please do not hesitate to call me if any additional information is required.

Very truly yours,


Evelyn F. Pugh
Director

EFP/C

cc: Mr. Robert W. Becker
Mr. Robert Mura
Mr. Frank E. Robin

TABLE 11 - PRELIMINARY SITE APPRAISAL
CITY OF NEW ORLEANS, ALGIERS BRAKE TAG STATION

Alternatiye Site 1a. On Lee's Lane (Wiltz) at Magellan Canal

ESTIMATE OF COSTS (Date of Value October 1982)

(a) <u>Lands & Damages</u>	<u>SF</u>	<u>Unit Value</u>	<u>Total Value</u>
Residential (Zoned Rd-2)	13,800	\$1.00	\$13,800
Improvements			10,000
Severance Damage			<u>30,000</u>
Total (R)			\$54,000
Contingencies 25% (R)			14,000
 (b) <u>Acquisition Costs</u> (Estimated 1 tracts)			
Non-Federal	1 @ \$1,400 per tract		1,000(R)
Federal	1 @ 700 per tract		1,000(R)
 (c) <u>PL-91-646</u>			
			0
(d) Total Estimated Real Estate Cost			<u>\$70,000</u>

TABLE 11a

Alternative Site 1b. Corner of General Meyer at Lee's Land (Wiltz)ESTIMATE OF COSTS (Date of Value October 1982)

(a) <u>Lands & Damages</u>	<u>SF</u>	<u>Unit Value</u>	<u>Total Value</u>
Commerical (Zoned B-1)	12,000	\$2.75	\$33,000
Improvements			0
Severance Damage			<u>7,500</u>
Total (R)			\$41,000
Contingencies 25% (R)			10,000
 (b) <u>Acquisition Costs</u> (Estimated 1 tracts)			
Non-Federal	1 @ \$1,400 per tract		1,000(R)
Federal	1 @ 700 per tract		1,000(R)
 (c) <u>PL-91-646</u>			
			0
(d) Total Estimated Real Estate Cost			<u>\$53,000</u>

TABLE 11b

Alternatiye Site 2* Wall Boulevard at Lee's Lane (Wiltz)

ESTIMATE OF COSTS (Date of Value October 1982)

(a) <u>Lands & Damages</u>	<u>SF</u>	<u>Unit Value</u>	<u>Total Value</u>
Residential (Zoned RS-2)	12,000	\$2.00	\$24,000
Improvements			0
Severance Damage			<u>24,300</u>
Total (R)			\$48,000
Contingencies 25% (R)			12,000
 (b) <u>Acquisition Costs (Estimated 1 tracts)</u>			
Non-Federal	1 @ \$1,400 per tract		1,000(R)
Federal	1 @ 700 per tract		1,000(R)
 (c) <u>PL-91-646</u>			
			0
 (d) Total Estimated Real Estate Cost			<u>\$62,000</u>

* This estimate is based on assumption that Lee's Lane is a dedicated street even though it is not constructed.

These estimates are based on Maps and area furnished by Engineering Division.

TABLE 11c

Existing Site (Land Only)

ESTIMATE OF COSTS (Date of Value October 1982)

(a) <u>Lands & Damages</u>	<u>SF</u>	<u>Unit Value</u>	<u>Total Value</u>
Commercial	11,364	\$8.00	\$90,912
Improvements			0
Severance Damage			<u>0</u>
Total (R)			\$91,000
Contingencies 25% (R)			23,000
(b) <u>Acquisition Costs</u>			0
(c) <u>PL-91-646</u>			0
(d) Total Estimated Real Estate Cost			<u>\$114,000</u>

Each of the three alternative sites provide utility and access equal to but no better than that at the existing site.

ALGIERS POINT BRAKE TAG STATION



Front View
Entrance on Patterson Street



Side View
Exit on Bermuda Street

FOR COMPARISON ONLY



Midcity Brake Tag Station, 4-Lane Capacity
Newest Facility of City



Stacking Lanes (4) at Midcity
Approx. 370 ft. long
2 Center Lanes with Median are 42' Wide